

WARD: Hartcliffe & Withywood **CONTACT OFFICER:** Thomas Wilkinson
SITE ADDRESS: Merchants Academy Gatehouse Avenue Bristol BS13 9AJ

APPLICATION NO: 18/03233/F Full Planning

DETERMINATION DEADLINE: 11 September 2018

Erection of a 2 form-entry Primary School with Nursery and Autistic Condition Spectrum (ASC) School (to be co-located on the site), with associated play areas, car parking and drop off area. Demolition of former St Johns Ambulance building to create new access and parking area from Hareclive Road. (Major application).

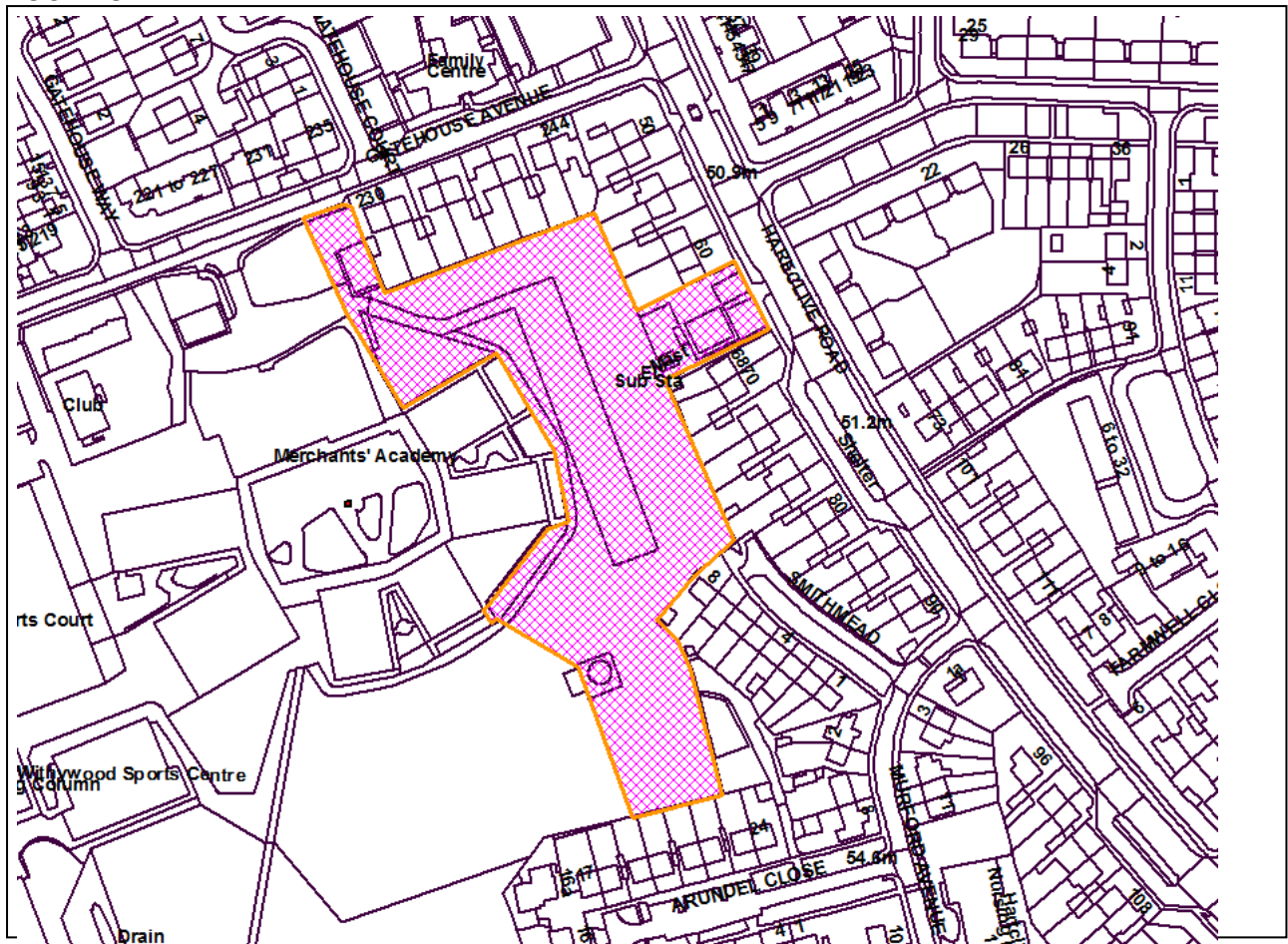
RECOMMENDATION: Grant subject to Condition(s)

AGENT: PCL Planning Ltd
1st Flr 3 Silverdown Office Park
Fair Oak Close
Clyst Honiton
Exeter
EX5 2UX

APPLICANT: Education And Skills Funding Agency
C/o Agent

The following plan is for illustrative purposes only, and cannot be guaranteed to be up to date.

LOCATION PLAN:



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BACKGROUND AND SUMMARY

This application is for the proposed relocation and expansion of the existing Merchants Academy Primary School and Venturers Academy (an Autistic Condition Spectrum (ACS) school) to create a new combined two-form entry primary school with a nursery and an expanded Autistic Condition Spectrum School on existing school land to the east of the existing Merchants Academy Secondary School site.

The existing Merchants Academy Primary School; nursery and Venturers Academy are currently located at a separate site on Withywood Road, approximately 600 metres to the south-west. The existing Merchants Academy Primary School has been identified under the Priority Schools Building Programme as beyond its life and requiring re-building. It is proposed to relocate these schools from their existing location to sit alongside the existing Merchants Academy Secondary School. Following relocation, the applicant has confirmed that the existing school buildings on this site will remain in use for educational purposes. The form of this is yet to be confirmed and will be subject to final approval by the Regional Schools Commissioner and Bristol City Council, however the applicant confirmed they envisage this would most probably involve vocational education linked to apprenticeships or some other alternative provision.

The existing capacity of the school is for 263 students, 38 nursery children and 70 Autism Resource Base pupils. Following development/expansion, the capacity of the new facilities will be for 420 students, 38 nursery children and 84 Autism Resource Base pupils.

A previous application at the site for a similar development (reference 17/03021/F) was refused by Development Control Committee A on 29th November 2017. The reason for refusal was due to Members concerns over the overshadowing, overbearing and overlooking impact of the development on surrounding residential properties, alongside the removal of 3.no category A trees from the site.

An appeal against this decision is currently pending, with a Hearing scheduled to take place on the 3rd October 2018.

Changes have been made to the proposed scheme and further information has been provided under the current application the in an attempt to address the previous refusal reason. The main changes can be summarised as follows:

- The proposed school building has been sited a further 1.6 metres from the north eastern boundary with Hareclive Road.
- Additional shadow studies have been prepared and provided to demonstrate the impact of the development in terms of overshadowing, having regards to existing conditions.
- An eyesight level plan has been provided to illustrate the use of high-level windows incorporated into the external elevations of the building.
- Additional opaque glazing is included
- Further justification has been provided as to why the new school building cannot be accommodated on the existing primary school site
- Further information/justification has been provided in relation to the removal of trees and compliance with the Council's Tree Replacement Standard.

In terms of the current planning application, objections were received from 11 surrounding residential properties. These were predominantly in relation to potential amenity issues (overbearing, overshadowing, overlooking) arising due to the height and proximity of the development in relation to surrounding properties, as well as potential highway safety and noise issues (see full details of the objections below and on the BCC website).

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Letters of support were also received from 34 properties.

The application has not been referred to Committee by any Councillor, but due to the level of public interest and the nature of the development including the issues arising and the site history it is considered appropriate for this application to come before Committee. Also, it is recognised that the development is very similar to that refused under previous application reference 17/03021/F and which is currently subject of the appeal mentioned above. It is considered that Committee should have an opportunity to consider the revised proposals prior to the appeal hearing so that the Inspector is aware of the Committee's view regarding the revised proposals in order to inform their consideration of the previous proposal.

Following a thorough assessment taking into consideration the revised plans and additional detail provided seeking to address the concerns of Members, alongside the revisions secured as part of the previous application it is considered that the amenity impact (overbearing, overshadowing and overlooking) whilst regrettable would on balance not warrant the refusal of a scheme that will deliver significantly important school places and enhanced teaching facilities. The loss of the 3.no category A trees is also considered to be acceptable, given the justification provided by the applicant and the significant on site mitigation planting. It is recognised that the Council's Arboricultural Officer also considers that these trees may even be Category B given their relatively small stem diameters.

Giving great weight to the need to create, expand or alter schools as required by the NPPF a balanced recommendation for approval is proposed to Members, subject to the conditions as set out below.

SITE DESCRIPTION

The application site refers to a section of existing school grounds within Merchant's Academy secondary school, to the east of the site. The site is currently characterised by a walled garden, hardstanding and some grassed areas used sporadically as ancillary teaching space for the secondary school and as car parking. The site also includes the adjoining former St Johns Ambulance building to the east of the site, accessed from Hareclive Road.

The application site forms part of an area of designated Important Open Space (which covers the entirety of the school playing fields) within the adopted Site Allocations and Development Management Policies Local Plan (July 2014).

The existing secondary school is comparatively recent in construction (being completed in 2008) and supports a number of sports facilities as well as teaching buildings.

The site is located in the Hartcliffe ward of South Bristol, approximately 4 miles south of Bristol City Centre. The site lies to the west of Hareclive Road and south of Gatehouse Lane, which provides the existing access to the secondary school. The site adjoins the existing secondary school to the west, with residential development at Hareclive Road/ Gatehouse Avenue/ Smithmead to the north, east and south. The wider area is also predominantly residential with associated community uses, including Gatehouse Centre, New Fulford Family Practice and Saint Pius Roman Catholic Church.

As set out above, the existing Merchants Academy Primary School; nursery and Venturers Academy are located at Withywood Road, approximately 650metres further west along Gatehouse Avenue. The existing Merchants Academy Primary School has been identified under the Priority Schools Building Programme as beyond its life and requiring re-building. It is proposed to relocate these schools from their existing location to sit alongside the existing Merchants Academy Secondary School.

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The Primary school currently has 263 students as well as 38 nursery children. The primary school moved to being 2-form entry school 3 years ago. Once the primary has 2 form entries in all years, the school would need capacity for 420 students. The current school building was not designed to be 2-form entry and could only cope with current student numbers by utilising some of the rooms that Venturers Academy now occupies.

Venturers Academy currently has 65 students and it is anticipated that there will be at least 70 by September 2017. This is the maximum number of students that can currently be accommodated in the current buildings, without additional capacity. The relocation to the secondary site will build extra capacity to take on at least 84 students. It is understood that there will be no increase in capacity of nursery children, with the numbers only transferring from the existing site.

An existing St Johns Ambulance building is currently located on Hareclive Road, between Nos.60 and 68, which was most recently used by local scout/cub groups. The applicant has confirmed that the building is currently disused, with the community groups who previously used the building now using alternative premises nearby.

RELEVANT HISTORY

APP/Z0116/W/18/3203776: Appeal of refused application reference 17/03021/F (see below). PENDING.

17/03021/F: Erection of a 2 form-entry Primary School with Nursery and Autistic Condition Spectrum (ASC) School to be co-located on the site, associated play areas, car parking and drop off area. Demolition of former St Johns Ambulance building to create new access and parking area from Hareclive Road. APPLICATION REFUSED on 29.11.2017 by DC Committee for the following reason:

'The proposed development by reason of its overall design, scale, bulk, form and massing in close proximity to neighbouring residential properties to the north (Gatehouse Avenue), east (Hareclive Road) and south (Smithmead) would result in unacceptable overshadowing; would result in harmful levels of overlooking; and would have an overbearing impact on these properties to the detriment of residential amenity. The overall form and design of the development would further result in the loss of 3.no category A trees within the site. The proposal would therefore be contrary to Policies BCS9 and BCS21 of the Bristol Core Strategy (2011), Policies DM17, DM27 and DM29 in the Site Allocations and Development Management Policies (2014) as well as guidance within the NPPF.'

10/04028/F: Construction of a workshop and a walled garden. GRANTED on 01.11.2010

09/00566/F: Erection of a new Cadet Centre. GRANTED on 07.05.2009

06/03024/F: New academy comprising two storey pavilions surrounding a courtyard and landscaping to replace existing Withywood Community School. GRANTED on 18.10.2006

06/01333/M: 'Reserved Matter' application for the approval of 'External Appearance' in association with the Merchants Academy (Outline Approval Ref No 05/02463/P/S). GRANTED on 26.05.2006

05/05043/M: Reserved matters application (relating to design and landscaping) for new city academy to replace Withywood Community School. GRANTED on 23.03.2006

05/02463/P: Application for outline planning permission to construct a new city academy to replace the existing Withywood Community School. GRANTED on 07.09.2005

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EQUALITIES ASSESSMENT

During the determination of this application due regard has been given to the impact of this scheme in relation to the Equalities Act 2010 in terms of its impact upon key equalities protected characteristics. These characteristics are age, disability, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sex and sexual orientation. There is no indication or evidence (including from consultation with relevant groups) that different groups have or would have different needs, experiences, issues and priorities in relation this particular proposed development. Overall, it is considered that the approval of this application would not have any significant adverse impact upon different groups or implications for the Equalities Act 2010.

APPLICATION

Planning permission is sought for the construction of a 2 form-entry Primary School with Nursery and Autistic Condition Spectrum (ASC) School to be co-located on the site within the main Merchants Academy site, alongside associated play areas, car parking and drop off area. Both of the schools will be located within the same building, with shared hall, library and other facilities at the central core of the building.

The proposal also involves the demolition of a former St Johns Ambulance building to create a new access and parking area from Hareclive Road between Nos.60 and 68. Extensive landscaping with the construction of new play areas, games court and new walled garden will also be provided.

STATEMENT OF COMMUNITY INVOLVEMENT

a) Process

The applicant has confirmed that as part of this application rather than being a wider community consultation drop in event (like that held in advance of the submission of the first application) the pre-submission community consultation focused on those neighbours closest to the proposed new building; those along Hareclive Road. The applicant has confirmed that ten neighbouring properties were invited to attend a meeting with the project manager, architect and head teacher on June 6th at 6pm. Further conversations were held with Ward Members on site and at City Hall.

b) Outcomes

In response to residents' concerns, the building was moved 1.6 further away from the homes (along Hareclive Road).

Previous application:

a) Process

As part of the initial (refused) application reference 17/03021/F the pre-application consultation that took place involved a public exhibition for the proposed scheme presented to the local community on May 22nd 2017 in the former St Johns Ambulance Hall, between 4pm - 7pm.

Staff and parents of Merchant's Academy Trust were advised of the event by the schools and details of the exhibition were also placed on the Merchant's Academy and Venturers Academy websites.

Invitations were also sent by post to 310 properties in the surrounding area, and by email to local councillors and local community groups (Hartcliffe and Withywood Pride of Place Group, Malago Valley Conservation Group, Bristol Civic Society Major Sites Group and the Neighbourhood Partnership Coordinator and Hareclive and Withywood Community Partnership).

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A total of 12 feedback forms were left on the date, with a further email sent to the planning consultant acting on behalf of the applicant the day after the event. A summary of the concerns raised are as follows:

- Concerns in relation to increased traffic congestion
- Concerns in relation to additional parking
- Concerns in relation to the proximity of the development to surrounding residential properties and subsequently loss of privacy and light
- Concerns in relation to noise/disturbance
- Concerns in relation to dust and noise disruption during construction
- Concerns in relation to structural integrity of works on surrounding properties
- Concerns in relation to increased littering
- General concerns in relation to the design of the building

b) Outcomes

- A Transport Assessment (TA) and Travel Plan (TP) have been prepared and submitted to support the application
- Travel surveys have been undertaken for the existing primary school which identify that the majority of children travel to school by non-car modes.
- A landscape buffer will be provided between the proposed school and the adjoining properties.
- High level strip windows have been incorporated into elevations to ensure that there will not be any issues in relation to overlooking or loss of privacy
- A shading analysis has also been undertaken to consider shadowing throughout the day
- An Environmental Noise Assessment has been prepared and submitted to support the application

RESPONSE TO PUBLICITY AND CONSULTATION

The application was advertised via press and site notices. Neighbours were also consulted via individual letters sent on 26th June 2018.

Objections were received from 11 surrounding properties, which in summary stated that:

- The new building is too close to surrounding properties, which will result in detrimental amenity issues and loss of privacy (overbearing, overlooking and overshadowing)
- The new building is too high, which will result in detrimental amenity issues and loss of privacy (overbearing, overlooking and overshadowing).
- The development would result in highway safety issues due to dangerous parking
- There is insufficient parking space in the local area to accommodate the development
- The public consultation undertaken by the applicant prior to submitting was inadequate
- The application should be void as there is already an appeal running in relation to the previously refused application
- The development would overshadow private gardens of surrounding properties
- There is sufficient space on the existing primary school site therefore this application is unnecessary
- The development would result in detrimental levels of noise and disturbance
- The development would result in mutual overlooking
- The existing pavements are not wide enough to accommodate the increase in number of pedestrians
- Alternative sites haven't been fully considered
- The development would result in a harmful loss of trees
- The letters of support should be discounted as they are submitted from people who don't live in close proximity to the proposed development
- The development could impact upon the stability of neighbouring properties during the

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- construction period
- The development would cause dirt and dust during the construction period

34 letters of support have also been received, which in summary stated that:

- The new facilities are vitally important for children of the city as there is enormous demand for specialist autism provision
- Bringing the primary school on site and creating an all through school facility will benefit all of the students and offer a higher quality educational service
- There is a need for the new school as there is a very high demand for school places locally
- The existing site is not adequate for redevelopment/expansion for a number of reasons
- The existing school building is not fit for purpose

OTHER COMMENTS

BCC Transport Development Management Team has commented as follows:-

'Principle

The site is well located in a residential area, with good access to transport links. In principle a school site would be acceptable in this location.

Local Conditions

The site will be accessed from Hareclive Road. There is good footway provision in the direct vicinity of the site and pedestrians are protected from passing traffic by highway verges. The carriageways are wide enough to carry passing traffic, although additional parking may prevent this.

Trip Generation

The presence of a new school will generate a significant number of trips, by many modes of transport. Existing trip rates (taken from the existing school's hands up survey results) suggest that the new/relocated schools could, once they are at capacity, generate in the region of an additional 160 car borne pupils. Some of these will be shared trips with siblings at both the school itself or the existing Merchants Academy Secondary School. The 38% who currently arrive by car is a relatively high proportion and there is capacity to reduce this through a strong culture of active travel, encouraged through a School Travel Plan. Nevertheless the impact will be substantially felt in the peak times, on a key public transport corridor, as the relocation of the school will have an impact in the direct vicinity of this site. It is therefore essential that measures are put in place to reduce the impact on resultant safety concerns and congestion arising from parking.

The following would be required:

Reduce in trips by car - the school will be required to create and implement a School Travel Plan (STP). This would need to be in place prior to the new site opening. It is recognised a Framework Travel Plan has been prepared by the Transport Consultants, but the school must have input into the final STP, as they will be responsible for implementing the measures. This would then be updated and monitored regularly to ensure that the measures are working. Such measures would include the provision of cycle parking, scooter parking, lockers and wet weather storage, road safety training, awareness, walking and cycling promotion, working with neighbours, and promotion of car sharing. This would also apply to staff, and include measures such as reviewing the marking system to allow staff not to feel the need to drive every day, the provision of showers and lockers to promote walking and cycling, and promotion of car sharing.

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Further advice can be found at Modeshift STARS, and from The Council's Active Travel to School Officer. A School Travel Plan would be secured by a condition.

Improve public transport facilities to encourage greater use - The application package pays attention to the accessibility to the site by public transport. Passenger Transport have advised that a new bus shelter will be required to encourage the use of the stops by additional families and staff. This will serve to reduce the number of cars on the Hareclive Road and reduce overall delays, particularly to public transport. A contribution of £10,000 is sought for a 3-bay reverse cantilever shelter. This can be secured through a UU.

Make the pedestrian environment safer - The existing crossing island will not have adequate capacity for a school of this size, and will need to be removed to accommodate the new vehicular access. A signalised crossing will be installed to allow for children to cross safely with minimal disruption to the capacity of the highway. This will be at the expense of the Applicant. A highway agreement will be required. A Statutory Notice to install a crossing will be required and the cost to the Council to prepare this will be £5395. A signalised crossing is also liable for commuted sums for maintenance.

The current low level bollards are insufficient to draw attention to the presence of a school and these will need to be replaced by Pencil Bollards, which are used on new school schemes throughout the city.

Deter driving - dropping off facilities do little to reduce congestion and can create further difficulties if used by a significant number of parents. They can create conflict and also act to encourage car use. It is therefore the Authority's stance not to allow such facilities for mainstream schools.

There is an operational requirement for a drop off facility for the ASC school as these children will have complex requirements. However, the Merchants school will have no vehicular access to the drop off facility. It will be necessary for the school to manage this, and a condition will be required to prevent this, unless with express permission from the school.

Remove parking hazards - a comprehensive review of waiting restrictions will be required in order to ensure safe and effective operation of the highway network. These could be in the form of single yellow lines or double yellow lines, peak hour loading prohibition and school keep clear markings where necessary. This will require a Traffic Regulation Order and highway works. The cost of a TRO is £5395 and will need to be met by the applicant. The TRO Process is a lengthy process requiring consultation with affected parties and urgent consideration is required to allow these restrictions to be designed, approved and advertised in order for the TRO to be sealed in adequate time for its installation.

Warn and slow down drivers - the school flashing wig wag signs will need to be relocated in Hareclive Road.

The 20mph speed limit will need to be applied to Hareclive Road on the approaches to the site. This will require a Traffic Regulation Order and highway works to signing to reflect this. The cost of a TRO is £5395 and will need to be met by the applicant.

The highway measures can be secured via a condition and will result in a highway agreement under s278 of the Highways Act. Means to secure the 3no TRO contributions are included within the highway agreement.

Access / Visibility

The proposed vehicular access is satisfactory and visibility is adequate. It will be essential to ensure that the access is used only by staff and those attending the Autism Hub, to reduce conflict directly outside the school.

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A wide, segregated pedestrian access is proposed. This is beneficial as it segregates all pedestrian access from any vehicular access.

A turning head is provided with minibuses using the top end of the turning circle to drop off. The swept paths provided show a minibus reversing in this area, not using the drop off facility, and in the process hitting one of the bollards. This area may need redesign to accommodate turning and dropping off effectively. Further details are required via condition. Further to this, swept paths showing minibuses passing at the access to Hareclive Road will also be required to ensure that the crossing is not obstructed by queuing vehicles waiting to access the site. Further information is required to show that this can work appropriately.

Parking and Servicing

The car parking for staff and visitors falls within the maximum standard for a school of this size. A School Travel Plan will assist in reducing the amount of on-street parking and waiting restrictions will reduce inappropriate parking.

Cycle parking is shown as 68 covered secure spaces which is acceptable. Scooter parking for 50 scooters is also provided.

Construction Management

A highway network construction management plan will be required and will be subject to a condition.'

Air Quality has commented as follows:-

'There won't be an air quality issue with this number of car parking spaces in this location.'

Arboricultural Team has commented as follows:-

'As discussed at City Hall the arboricultural impact assessment (AIA) that has been presented (Advanced Arboriculture June 2017) was also presented for refused application 17/03021/F which we commented on.

The AIA identifies the constraints presented by the trees and also trees that need to be removed but does not present BTRS calculations for proposed tree removals. I have calculated the BTRS liability as 43 replacement trees.

Several trees proposed for removal have been categorised as A (6, 8 and 9). There is a degree of subjectivity in the categorisation method and considering their relatively small stem diameters I think a B category is more likely for these trees.

I am mindful that the AIA was initially presented for application 17/03021/F which was refused but was found by us to be acceptable on arboricultural grounds.

The AIA is unchanged since the previous application, as is the proposals in relation to the trees, therefore I currently have no objection on arboricultural grounds.

The landscape plan shows that 32 trees are proposed for planting along with 15 forest school trees and 6 fruit trees. This proposed planting is in excess of the BTRS liability.

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As tree protection has been specified within the arboricultural method statement plans and as tree planting is proposed as mitigation for tree removals I therefore raise no objections, subject to conditions.'

Contaminated Land Environmental Protection has commented as follows:-

'We refer to the major planning application to develop a new primary school on the site of the Merchants Academy, Gatehouse Avenue. We have reviewed the following submitted reports as part of the application

- Ground and Water. March 2017. Merchants Academy Primary School & Ventures Academy ASC School. Desk Study. GWPR1867/DS/March 2017. V1.01. Final
- Ground and Water. March 2017. Merchants Academy Primary School & Ventures Academy ASC School. Ground Investigation Report. GWPR1867/GIR/March 2017. V1.01. Final

Overall the reports submitted adequately assess risks from contamination. The only potential concern we have with the assessment is adopting Public Open Space Residential scenario for the area of the school buildings themselves as the chosen scenario does not consider the risks of vapour intrusion from hydrocarbons and volatile organic compounds. However the results of the soil samples from both 2017 and a historic report held within the office do not indicate an issue with vapour intrusion from these contaminants.

Further assessment of contamination is not required but given previous experience of school sites of this age with encountering unexpected contaminants in the site soils we do recommend that any approval is subject to a condition requiring that the applicant reports and unexpected contamination immediately to the Local Planning Authority.

We do note section 7.9 refers to imported material needing to be tested which we concur with, can the applicants confirm how much imported topsoil is likely to be bought onto site (an estimate will suffice) for the proposed landscaped areas? If significant we are minded to apply a verification of imported materials condition.'

Crime Reduction Unit has commented as follows:-

'No objections'

Nature Conservation Officer has commented as follows:-

'Trees will be removed as part of this proposal. All species of wild birds, their eggs, nests and chicks are legally protected until the young have fledged. I therefore recommend a condition is attached to any approval requiring that there should be no clearance of vegetation or structures suitable for nesting birds between 1st March and 30th September (nesting season) without the prior written approval of the local planning authority. No further ecology issues are identified.'

Pollution Control has commented as follows:-

'I can confirm that I am happy with the contents of the submitted acoustic reports, which deal with the internal noise levels for teaching within the classrooms and the potential for noise nuisance from external plant and equipment, as well as potential noise from children in outside areas and noise from cars and children leaving the school. I would just like to make sure that the recommendation made within the reports that a 2.4 m high acoustic barrier be installed along the northern and southern boundaries occurs prior to the use commencing. It should be noted that with the barrier noise from children in the outdoor areas will still be audible at nearby residential properties, however I do not consider that this would be harmful enough to warrant the refusal of the application. I would however ask that an Outdoor Area Management Plan is provided, setting out details of times the external areas

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will be used, numbers of children that will use specific areas at any one time and how the areas will be supervised to ensure any impact is minimised.

I also have concerns regarding noise from construction works, noise from construction vehicles and the potential for dust and mud on roads for construction activities. I would therefore ask that a site specific Construction Environmental Management Plan is provided.

I would also like to see some time restrictions regarding the community use and feel that these should be in line with the current use for the school that secondary facilities are currently offered to external parties from 5pm until 10pm on weekdays and from 8am until 9pm. The external areas should also not be used for community purposes after 6pm.

Detail of any extraction and ventilation systems should be provided to the Council prior to installation to ensure they will result in no harmful odour or noise issues arising.'

Sport England has commented as follows:-

'It is understood that the proposal prejudices the use, or leads to the loss of use, of land being used as a playing field or has been used as a playing field in the last five years, as defined in The Town and Country Planning (Development Management Procedure) (England) Order 2015 (Statutory Instrument 2015 No. 595). The consultation with Sport England is therefore a statutory requirement.

Sport England has considered the application in light of the National Planning Policy Framework (particularly Para 74) and Sport England's Playing Fields Policy, which is presented within its Planning Policy Statement titled 'A Sporting Future for the Playing Fields of England' (see link below): www.sportengland.org/playingfieldspolicy

Sport England's policy is to oppose the granting of planning permission for any development which would lead to the loss of, or prejudice the use of, all/part of a playing field, unless one or more of the five exceptions stated in its policy apply.

Having assessed the application for the erection of a 2 form-entry Primary School with Nursery, Sport England are satisfied that the proposed development meets the following Sport England Policy exception:

E3 - The proposed development affects only land incapable of forming, or forming part of, a playing pitch, and does not result in the loss of, or inability to make use of any playing pitch (including the maintenance of adequate safety margins), a reduction in the size of the playing area of any playing pitch or the loss of any other sporting/ancillary facility on the site.

This being the case, Sport England does not wish to raise an objection to this application.'

Flood Risk Manager has commented as follows:-

'The proposed drainage strategy is acceptable and we have no objection to the proposals. We request that should planning permission be granted our standard pre commencement condition is imposed to ensure a detailed drainage design formed in accordance with the approved strategy is submitted to and approved by BCC prior to the commencement of development.'

Sustainable Cities Team has commented as follows:-

'The use of thermal modelling and the CIBSE TM52 methodology is noted and welcome, as is the proposal to install a PV array to reduce residual emissions. There is no district heating network in the vicinity of the development.

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The pre-assessment estimate of BREEAM Very Good is noted as are the reasons why 'Excellent' is regarded as unachievable. I suggest conditioning a report from Arcadis at completion stage and prior to occupation confirming that development as designed will achieve BREEAM 'Very Good'.

An adequate Sustainable Urban Drainage scheme should be secured via condition. The inclusion of dedicated cycle parking is noted and welcome.

Following the submission of further detail and subject to conditions I recommend the application be approved.'

Urban Design has commented as follows:-

'It is recognised that the design principles of new school building have been heavily influenced by the existing secondary school, the shape of the site and surrounding context (including residential amenity) and the requirements for such teaching spaces/floorspace needs. The overall siting, form and layout of the development is considered acceptable in this respect.

In scale, the building will consist of one and two storey masses. The proposed building will be visible from public view, particularly from Hareclive Road to the east where it will be marginally visible above the roofs of the residential properties which line the street, and will be visible through the gaps between houses. However, it is considered that the overall scale and massing will not be significant enough to cause any detrimental harm to the character and appearance of the area in this instance, as the building would only be marginally taller than surrounding residential properties.

The overall design and materials will further be relatively simple and of a suitable quality, to match and tie in with the material treatment of the adjacent existing secondary school. The overall scale and detailed design of the building is subsequently considered acceptable, subject to further detail of design features and material samples (secured via condition).

For safety and security reasons guard railings are proposed to the roof of the building. Whilst not ideal in design/visual terms, these railings will be collapsible so that when not in use they will not be erected. Subsequently due to the collapsible design and the limited timescale during which the railings will be erected (secured via condition) it is not considered that their presence on the roof of the building will cause enough harm to warrant a refusal on design grounds.'

Landscape has commented as follows:-

'The application fulfils an important educational need and the principle of the development is not contested in relation to landscape design. No objections to the application, subject to amendment and the submission of further detail.'

City Design Group Public Art has commented as follows:-

'Comments were provided at pre app submission stage after which discussions took place with the applicant regarding integration of public art across the development.

An art consultancy was engaged and work is progressing which at the time of this memo is focused on the landscape scheme with an artist working with HED the appointed landscape architects on the scheme design. The public art then will be the scheme landscape, including the entrance at Hareclive Road which was identified by City Design Group as benefiting from increased legibility. The artist is also involved in discussions about the interior colour / fit out.

A landscape scheme has been submitted as part of the full planning but as noted in the planning statement this landscape scheme will be updated during determination (as design work completes) with new drawings that incorporate the public art submitted prior to determination.

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A public art condition will also be applied to secure public art as part of the scheme and in pursuance of said landscape scheme, and will be passed to the case officer.

Further information on the BCC public art programme can be obtained at www.aprb.co.uk

RELEVANT POLICIES

National Planning Policy Framework – July 2018

Bristol Local Plan comprising Core Strategy (Adopted June 2011), Site Allocations and Development Management Policies (Adopted July 2014) and (as appropriate) the Bristol Central Area Plan (Adopted March 2015) and (as appropriate) the Old Market Quarter Neighbourhood Development Plan 2016 and Lawrence Weston Neighbourhood Development Plan 2017.

In determining this application, the Local Planning Authority has had regard to all relevant policies of the Bristol Local Plan and relevant guidance.

KEY ISSUES**(A) PRINCIPLE OF DEVELOPMENT**

As set out above, planning permission is sought for the construction of a 2 form-entry Primary School with Nursery and Autistic Condition Spectrum (ASC) School to be co-located on the site within the existing main Merchants Academy secondary school site, alongside new associated play areas, car access, parking and drop off area. The development will represent the relocation and expansion of existing school facilities located approximately 600 metres away to the south-west. Both of the schools (Merchants and Venturers Academy) will be located within the same building, with shared hall, library and other facilities. The proposal also involves the demolition of a former St Johns Ambulance building to create the new access and parking area from Hareclive Road.

Paragraph 94 of the National Planning Policy Framework (2018) states that it is important that a sufficient choice of school places is available to meet the needs of existing and new communities. Local planning authorities should take a proactive, positive and collaborative approach to meeting this requirement, and to development that will widen choice in education. The NPPF states that Local Planning Authorities should:

- Give great weight to the need to create, expand or alter schools through the preparation of plans and decisions on applications; and
- Work with schools promoters, delivery partners and statutory bodies to identify and resolve key planning issues before applications are submitted.

The principle of the development and expansion of the primary school and nursery is therefore supported in national planning policy terms in this respect.

In local policy terms, Bristol Core Strategy (2011) Policy BCS12 sets out the general approach to the protection and development of community facilities (which includes schools). This policy states that community facilities should be located where there is a choice of travel options and should be accessible to all members of the community and where possible community facilities should be located within existing centres. Existing community facilities should in addition be retained, unless it can be demonstrated that there is no longer a need to retain the use or where alternative provision is made.

Policy DM5 in the Site Allocations and Development Management Policies (2014) sets out that the term community facilities is wide-ranging and can include community centres, childcare facilities, education establishments, training centres, health and social care facilities and civic and

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administrative facilitates. Policy DM5 states that proposals involving the loss of community facilities land or buildings will not be permitted unless it is demonstrated that:

- i. The loss of the existing community use would not create, or add to, a shortfall in the provision or quality of such uses within the locality or, where the use has ceased, that there is no need or demand for any other suitable community facility that is willing or able to make use of the building(s) or land; or
- ii. The building or land is no longer suitable to accommodate the current community use and cannot be retained or sensitively adapted to accommodate other community facilities; or
- iii. The community facility can be fully retained, enhanced or reinstated as part of any redevelopment of the building or land; or
- iv. Appropriate replacement community facilities are provided in a suitable alternative location.

It is acknowledged that the existing Merchants Academy Primary School and Venturers Academy has been identified under the Priority Schools Building Programme as beyond its life and requiring re-building. The applicant has also confirmed that it would be difficult to achieve the desired increase in capacity at the current, existing site given the limited space available and site constraints. The proposal to relocate/replace these schools from their existing location to sit alongside the existing Merchants Academy Secondary School is subsequently acceptable as there will be no loss of school/community floorspace in the local area, with the proposed development resulting in improved and expanded educational facilities for the local community with a greater capacity for pupils.

The entire site is already in current educational use. No change of use or alteration of the site boundary is proposed; and thus the site is considered an acceptable location for the provision of a replacement educational facility, subject to meeting all other relevant policy requirements (see below).

The proposed development would however involve the demolition of another existing community building which was formerly owned by the St Johns Ambulance, although most recently has been used by local scout/cub groups. The applicant has however confirmed that the community groups who previously used this building are now using alternative premises in the local area. Whilst the loss of this building is not ideal, it is recognised that the proposed new school development will include two halls, studio and training rooms which could be used outside of school times by the community in the same way that existing facilities at the secondary school are currently used by the wider public. The overall floorspace created by these new facilities will be greater than the floorspace of the existing community building. In addition, as set out the community groups that use the building have relocated to a suitable alternative location. On this basis, the loss of the community building is considered acceptable as appropriate replacement community facilities will be provided in suitable alternative locations; in line with criterion iv of Policy DM5. Further, in visual terms the existing St Johns Ambulance building itself is not considered to hold significant architectural merit, being a single storey in height and primarily functional in appearance, meaning it doesn't contribute overly positively towards the character and appearance of the area or streetscene in this instance and its loss is thus acceptable in design terms.

As the application site forms part of an area of designated Important Open Space within the adopted Site Allocations and Development Management Policies (2014), Policy DM17 in this document applies. This states that development on part, or all of an important open space as designated will not be permitted unless the development is ancillary to the open space use. Policy BCS9 of the Bristol Core Strategy (2011) is also applicable, and states that the integrity and connectivity of the strategic green infrastructure network should be maintained, protected and enhanced. Open spaces which are important for recreation, leisure and community, townscape and landscape quality and visual amenity should be protected.

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In this instance it is considered that the area of land removed to accommodate the development does not realistically fulfil an important open space function as it comprises land which is predominantly hard standing and used informally as car parking. The majority of the designated open space to the south of the development site, is however considered to hold much greater amenity value, and will remain undeveloped and thus would not be significantly impacted upon by the proposals. The overall function, integrity, connectivity and primary character of the open space would fundamentally remain and therefore would not be materially or detrimentally impacted upon by the proposal to warrant refusal on this ground. Overall whilst not ideal given the loss of some open space, the location of the new school building is in principle considered acceptable.

The applicant has also advised that other location options were considered prior to the further progression of proposals, however ultimately it was assessed that the identified site represents the most suitable for the proposed expansion and relocation of the existing school for a number of reasons as follows:

- The existing school site (located approximately 600 metres to the south-west) has been identified under the Priority Schools Building Programme as beyond its life and requiring re-building and it would be difficult to achieve the desired increase in capacity at the current site given the limited available space
- Locating all elements of the Merchants Academy school on one site would improve cross-phase working and sharing of resources and facilities
- Being co-located with a mainstream setting allows for true inclusion practices to take place. Young people with disabilities can access more than one provision without any barriers, cost or delay.
- The identified area of the school site is largely disused as car parking and a walled garden area of no particular visual merit
- The expansion in any other location within the school site would likely impact negatively upon the designated Important Open Space and would also compromise the playing and sporting facilities.
- The approach provides an opportunity for the existing site to be vacated, allowing it to be redeveloped by the existing freeholder.
- The existing site does not offer enough space to be safely redeveloped whilst being occupied by the current school(s)

Given the above, the overall principle of development of the site is considered acceptable and is supported.

(B) DO THE PROPOSALS RAISE ANY SPORTS AND LEISURE PROVISION ISSUES?

The proposed development prejudices the use of land currently being used as a playing field (or has been used as a playing field in the last five years) as defined in The Town and Country Planning (Development Management Procedure) (England) Order 2015 (Statutory Instrument 2015 No. 595). The consultation with Sport England is therefore a statutory requirement and has been undertaken.

Following consultation, Sport England commented that they are satisfied with the application, as the proposed development affects only land incapable of forming, or forming part of, a playing pitch, and does not result in the loss of, or inability to make use of any other playing pitch (including the maintenance of adequate safety margins), a reduction in the size of the playing area of any playing pitch or the loss of any other sporting/ancillary facility on the site. The application is subsequently considered acceptable on this basis.

(C) WOULD THE PROPOSAL BE ACCEPTABLE IN DESIGN TERMS?

Bristol Core Strategy Policy BCS21 (2011) advocates that new development should deliver high quality urban design that contributes positively to an area's character and identity, whilst safeguarding

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the amenity of existing development.

Policy DM26 in the Site Allocations and Development Management Policies (2014) expands upon BCS21 by outlining the criteria against which a development's response to local character and distinctiveness will be assessed. Development will not be permitted where it would be harmful to local character and distinctiveness or where it would fail to take the opportunities available to improve the character and quality of the area and the way it functions. Policy DM27 in the same document expresses that the layout, form, pattern and arrangement of streets, buildings and landscapes should contribute towards to creation of quality urban space and that the height, scale and massing of development should be appropriate to the immediate context, site constraints, character of adjoining streets and spaces and setting with Policy DM29 further stating that the design of new buildings should be of high quality.

The Council's City Design Group (CDG) has reviewed the proposals noting that the application fulfils an important educational need and overall they have raised no objections to the development in design terms. It is recognised that the overall design of the proposed new school (including boundary treatment, landscaping etc.) is the same as that proposed under previous application reference 17/03021/F; the overall design of which was considered acceptable by officers and committee members during the determination of the application. As per the previous application, the design principles of the new school building have been heavily influenced by the existing secondary school, the shape of the site and surrounding context (including residential amenity) and the requirements for such teaching spaces/floorspace needs.

The new schools will be located within a single building which consists of two wings arranged around a central core of shared facilities and framing two external courtyard play spaces to the west which have direct connectivity between the existing and proposed schools on the site. The existing site, whilst relatively large is of an awkward shape and is surrounded by a number of residential properties with rear gardens which back onto the development site. In this respect, the building has been sited to provide as much boundary distance as possible between residential properties and the proposed school, and also to form a protective enclosure between the external play areas to the west of the site and residential gardens. The overall siting, form and layout of the development is considered acceptable in design terms.

In scale, the building will consist of a varied massing of one and two storey elements. The reduced one storey element at the centre of the building is included to provide both a link and a contrast to the two two-storey blocks to either side. A one-storey block for the nursery is also proposed to the south side of the site.

The proposed building will be visible from public views, particularly from Hareclive Road to the east where it will be marginally visible above the roofs of the residential properties which line the street, and through the gaps between houses. However, it is considered that the overall scale and massing will not be significant enough to cause any detrimental harm to the character and appearance of the area in urban design terms in this instance, as the building would only be marginally taller than surrounding residential properties. The overall design and materials will also be relatively simple and of a suitable quality, to match and tie in with the material treatment of the adjacent existing secondary school. The overall scale and detailed design of the building is subsequently considered acceptable, subject to further detail of design features and material samples (secured via conditions as set out below).

For safety and security reasons, guard railings are proposed to the roof of the building. Whilst these are not of the best quality in design/visual terms, following Case Officer advice the applicant has confirmed that these railings could be collapsible so that when not in use for maintenance purposes they will not be erected, thus minimising their visual impact. Further details of these railings are secured via condition to ensure they will be of an appropriate appearance.

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The location of the car parking to the front of the building does not provide an ideal entrance design statement to the school site with regard to good design principles. It is recognised however that there is a need for parking and it is accepted that there are no other suitable alternative locations within the site. The school will be an important new building on the Hareclive Road frontage and the entrance merits particular consideration in relation to the quality of its detailed treatment. Initial concerns were raised by the Council's Landscape Officer during the course of the previous application in relation to the boundary treatment as originally proposed, as it was not considered of sufficient visual appeal. Subsequently following Case Officer advice the applicant provided revised entrance, boundary treatment and landscaping plans; which the Council's Landscape Officer reviewed and has confirmed that the new street frontage will be of an acceptable quality. The Council's Landscape Officer also confirmed that all the landscaping and surfacing within the site will also be of a suitable quality. Whilst trees are proposed to be removed to facilitate the development, replacements will be planted on site in accordance with the Bristol Tree Replacement Standard (further detail on this is set out in Key Issue F below). The landscaping and tree replacement proposals are also secured by relevant conditions.

Finally, as the application proposal is a 'major' scale planning application, Policy BCS21 of the Bristol Core Strategy states that major development should deliver high quality urban design and enable the delivery of permanent and temporary public art. Accordingly the applicant has provided a concept public art plan which details a number of proposed elements of public art as part of the development. This is considered acceptable, with further detail secured via condition which is set out below with all the other conditions required above.

(D) IMPACT ON THE AMENITY OF SURROUNDING PROPERTIES

Policy BCS21 in the Bristol Core Strategy (2011) advocates that new development should deliver high quality urban design and safeguard the amenity of existing development. Policies DM27 and Policy DM29 in the Site Allocations and Development Management Policies (2014) further state that the layout and form of new buildings and development, including the size, shape, form and configuration of blocks and plots, will be expected to enable existing and proposed development to achieve appropriate levels of privacy, outlook and daylight. Policy BCS23 in the Core Strategy and Policy DM35 in the Site Allocations and Development Management Policy also state that new development should also not lead to any detrimental increase in noise levels.

It is acknowledged that the siting of the new school building is not ideal as it will be located in close proximity to a number of surrounding residential properties to the north (Gatehouse Avenue), east (Hareclive Road) and south (Smithmead).

A previous application at the site for a similar development (reference 17/03021/F) was refused by Development Control Committee A on 29th November 2017. The main concern of Members which formed the reason for refusal was the perceived harmful overshadowing, overbearing and overlooking impact of the development on surrounding residential properties.

Changes have been made to the proposed scheme and further information has been provided under the current application in an attempt to address the previous refusal reason and concerns. The main changes can be summarised as follows:

- The proposed school building has been sited a further 1.6 metres from the north eastern boundary with Hareclive Road.
- Additional shadow studies have been prepared and provided to demonstrate the impact of the development in terms of overshadowing, having regards to existing conditions.
- An eyesight level plan has been provided to illustrate the use of high-level windows incorporated into the external elevations of the building and additional opaque glazing is included
- Further justification has been provided as to why the new school building cannot be

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accommodated on the existing primary school site

Justification for the Proposed Siting

The applicant has confirmed that other locations were considered for the siting of the new school building (including the existing primary school site) however ultimately the identified location represents the most suitable for the proposed expansion and relocation for a number of reasons:

- The existing school site (located approximately 600 metres to the south-west) has been identified under the Priority Schools Building Programme as beyond its life and requiring re-building and it would be difficult to achieve the desired increase in capacity at the current site given the limited available space
- Locating all elements of the Merchants Academy school on one site would improve cross-phase working and sharing of resources
- The identified area of the school site is largely disused as car parking and a walled garden area of no particular visual merit
- The expansion in any other location within the school site would likely impact negatively upon the designated Important Open Space and would also compromise the playing and sporting facilities.
- The approach provides an opportunity for the existing site to be vacated, allowing it to be redeveloped by the existing freeholder.
- The existing site does not offer enough space to be safely redeveloped whilst being occupied by the current school(s)

In this respect, the rationale behind the proposed siting of the new school building is understood. However, the impact of the school on the neighbouring residential premises to the north (Gatehouse Avenue), to the east (Hareclive Road) and to the south (Smithmead) needs to be carefully assessed as whilst the proposed new school building is only two storeys in height it would be taller than the total ridge heights of surrounding houses by approximately 2 metres, is sited in relatively close proximity to the neighbouring boundaries and due to the overall size of the building, would be of significant massing. The amenity impact on these surrounding properties is set out below.

Overshadowing to properties on Gatehouse Avenue (north)

To the north, the rear gardens of properties along Gatehouse Avenue will back directly onto the boundary of the proposed new school site. The new school building will be sited approximately 23 metres from the rear elevations of these properties. It is considered that the overshadowing impact on these properties will be the same as that under previous application reference 17/03021/F.

The applicant has provided a number of shadow plans to support the proposal. This demonstrates that in June the new development would not overshadow the rear gardens of properties along Gatehouse Avenue at any time of day. In September however the development would overshadow the majority of the rear gardens of three properties during the morning (9am) however this overshadowing would not impact upon the rear elevations of the dwellings themselves, and by 1pm only a small section of the gardens of three properties would be overshadowed. In December it is evident that the development would overshadow the rear gardens and properties along Gatehouse Avenue during the morning, however by 1pm the overshadowing impact would only be felt by two properties, and by 3pm only one property would be overshadowed, with a section of the rear garden of another property. In March the development would overshadow the rear gardens of three properties in the morning, however by 1pm and for the rest of the day the overshadowing would only impact upon a small section of the gardens of these three properties.

Overshadowing to properties on Hareclive Road (east)

To the east, the rear gardens of properties along Hareclive Road will back directly onto the boundary

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of the proposed new school site. Following the refusal of the previous application (17/03021/F) the applicant has sought to improve the relationship between the development and properties to the east by siting the new school building a further 1.6 metres from the north eastern boundary with Hareclive Road. It is considered that this amendment has reduced the overshadowing impact on these properties.

The new school building will be sited between approximately 21 metres (at the closest point) and 25 metres (at the furthest point) from the rear elevations of these properties. The provided shadow plans demonstrates that in June the development would result in a very small section at the very bottom of the rear gardens of the properties along Hareclive Road being overshadowed, only during the afternoon (4pm). In September the development would again only result in the overshadowing of rear gardens during the late afternoon, with the rear elevations of the properties themselves not being impacted upon. In December the development would overshadow the rear gardens and elevations of properties along Hareclive Road, however this would only be during the late afternoon (4pm) with no overshadowing occurring to gardens or properties for the rest of the day. In March the development would result in approximately 50% of the rear gardens being overshadowed, only during the late afternoon (4pm).

Overbearing to properties on Gatehouse Avenue (north) and Hareclive Road (east)

As noted above, the new school building will be sited approximately 21 metres (at the closest point) and 25 metres (at the furthest point) from the rear elevations of properties along Gatehouse Avenue and Hareclive Road. It is accepted that given this proximity and given the overall scale and extent of development across the rear boundaries of these residential houses an overbearing impact and change of outlook will be experienced by these properties. However, on balance it is considered that the overbearing impact would not be harmful enough to warrant refusal; the separation distances of 21 to 25 metres is considered sufficient in this instance to ensure adequate outlook from the rear elevations of the properties. In this instance, the overbearing impact will be less significant and harmful than that under previous application reference 17/03021/F given the siting of the new building 1.6 metres further away from the rear elevations of properties along Hareclive Road.

Conclusion: Overbearing and Overshadowing on Gatehouse Avenue (north) and Hareclive Road (east)

It is evident following the above that whilst the development would result in some overshadowing of surrounding properties to the north and east this overshadowing would impact mainly upon sections of rear gardens rather than the properties themselves, during certain times of the year and during certain times of the day. Whilst there would be some overshadowing of the properties themselves this would only occur during winter months (December) at the end of the day when it is likely to be getting dark in any event.

It is evident that under the current application the overshadowing and overbearing impact on properties to the north (Gatehouse Avenue) will be the same as that under previous application reference 17/03021/F. However, the overshadowing and overbearing impact on properties to the east (Hareclive Road) will be less harmful than the previous proposal given the siting of the new building 1.6 metres further away from the rear elevations of properties along Hareclive Road.

Subsequently, the overbearing and overshadowing impact on properties to the north (Gatehouse Avenue) and east (Hareclive Road) is recognised, however on balance this is not considered harmful enough to warrant the refusal of the application on amenity grounds in this instance when also balancing the wider public benefits of the development in securing much need school places and giving great weight as required to the need to create, expand or alter schools through the preparation of plans and decisions on applications as set out in the NPPF.

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To the south, the end terrace property of Smithmead (Number 8) will be sited directly along the boundary with the development site so that the side elevation of the property will be approximately 1.5 metres from the development site boundary and approximately 6 metres from the overall new school building at the closest point. It is recognised that this property includes no windows to the side elevation which will directly face the new school building. The new school building under the current application has been sited a further 1.6 metres from the north eastern boundary with Hareclive Road than proposed under previous application reference 17/03021/F. This has reduced the overshadowing and overbearing impact on those properties (Hareclive Road) as noted above. However, as a result of this set back a greater mass of building will be sited along the rear boundary with No.8 Smithmead. Despite this greater massing of built form along the rear boundary it is however evident from the shadow studies provided that the overshadowing impact will be minimal and restricted late afternoon during the summer when the sun will be setting in any case, given the southerly orientation of this dwelling. This is considered acceptable.

Overbearing to Smithmead (south)

Given the height, massing and siting of the proposed development in such close proximity to this residential property, concerns were raised by the Case Officer during the course of the previous application (reference 17/03021/F) that the proposal would result in harm to residential amenity by virtue of an overbearing impact. Following Case Officer concerns, the scheme was subsequently amended during the course of the previous application so that the new building was brought away from the boundary by a greater extent and chamfered at the corner in order to improve the outlook from the front of the property and to seek to reduce the impact of the building as much as possible.

Following this amendment the new building will still be sited approximately 6 metres away from the side boundary to No.8 Smithmead. This distance is only measured however from the closest point between the two buildings (measured from the front elevation of No.8 Smithmead) with the majority of the building being sited further away from the boundary following the revisions submitted. To limit the overbearing impact further the new school building was also revised during the course of the previous application in this location so that the rear section of the wing adjacent to No. 8 Smithmead would be single storey in height.

It is recognised under this application in order to address the previous refusal reason the school building has been sited a further 1.6 metres from the north eastern boundary with Hareclive Road than previously proposed. This has reduced the overshadowing and overbearing impact on those properties (Hareclive Road) as noted above. However, as a result of this set back a greater mass of building will be sited along the rear boundary with No.8 Smithmead, including a greater massing of two storey built form. Despite this greater massing of built form along the rear boundary, whilst it is recognised the development would have an increased overbearing impact on No.8 Smithmead the Local Planning Authority remains satisfied that the impact, whilst not ideal, is not harmful enough to warrant refusal on balance. The alterations secured during the previous application have reduced the overbearing impact on the rear garden area of the residential property and also allowed sufficient outlook from rear windows. It is subsequently concluded that the rear outlook from this property would remain adequate, as there would not be such a considerable amount of built form directly along the rear garden boundary to cause any harmful overbearing impact, despite the additional set back.

The side elevation of No.8 Smithmead doesn't contain any windows which could be directly impacted upon by the development; however the overall bulk and massing of development would be apparent particularly from the front elevation. To reduce the impact of the bulk/massing as much as possible, following Case Officer advice the corner of the new school building closest to the boundary at the front elevation of No.8 Smithmead was chamfered as part of the previous application. It was previously considered that following this amendment the outlook from the front elevation would on balance be adequate, as an acceptable line of sight would be afforded in both directions. In this instance it is

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considered that the overbearing impact of the development on the front elevation of No.8 Smithmead will be improved, with the entire building being set back from the north eastern boundary by 1.6 metres reducing the amount of visible built form visible from the front windows.

Conclusion: Overbearing and Overshadowing on Smithmead

Following the above, it is recognised that the development would still have an overbearing impact on No.8 Smithmead given the close proximity and overall bulk/massing of the building. In addition, given the 1.6 metre set back from the north eastern boundary it is recognised that a greater massing of built form will be sited along the rear boundary, which is not ideal. However it is also considered that the impact on the front elevation will be improved given the reduction in visible massing from the front windows given the 1.6 metre set back. Given the above, and given the previous revisions secured as part of application reference 17/03021/F in an attempt to address Case Officer concerns (the scheme being revised in scale, design and siting to limit the overbearing impact as much as possible whilst delivering the required accommodation needed for the school) on balance it is concluded that the overbearing impact on No.8 Smithmead is not harmful enough to warrant refusal in this instance taking into consideration the nature of development which will fulfil an important educational need for the local community and again giving great weight to the need to create, expand or alter schools through the preparation of plans and decisions on applications as set out in the NPPF.

Overlooking to properties on Gatehouse Avenue (north) and Hareclive Road (east)

As noted above, to the north and east, the rear gardens of properties along Gatehouse Avenue and Hareclive Road will back directly onto the boundary of the proposed new school site. The new school building will be sited approximately 21 metres (at the closest point) and 25 metres (at the furthest point) from the rear elevations of these properties along Gatehouse Avenue (to the north) and Hareclive Road (to the east). The school will contain windows at ground floor level; however these will not offer direct views of surrounding properties given the presence of existing and proposed boundary treatment (secured by conditions).

At first floor level, the school will contain a number of windows which will face surrounding properties; however the school has been designed so many of these windows do not serve main teaching rooms but rather circulation space and back-office functions. Obscure glazing is proposed to some windows to teaching areas where the overlooking impact has been identified as most sensitive (this is secured by condition). Further, under this application all windows which will directly face surrounding properties which are not obscure glazed will be high level, offering adequate natural daylight however not offering the opportunity for overlooking. An eyesight level plan has been provided to demonstrate this. Given the above, and given the separation distance of 21-25 metres it is considered that any overlooking or loss of privacy created between the new development and surrounding properties along Gatehouse Avenue and Hareclive Road will not be overly significant or detrimental enough to warrant refusal.

Overlooking to Smithmead (south)

To prevent any overlooking occurring between the development and properties to the south along Smithmead all windows to the south elevation of the new school will be obscure glazed as shown on the approved plans (also secured via condition). It is accepted however that the perception of overlooking will remain given the presence of windows, however as noted above the new school building will be mainly a single storey in height where it directly abuts the rear garden of No.8 Smithmead along the boundary, meaning the perception of overlooking of the rear garden would not be overly significant. In addition, No.8 Smithmead doesn't contain and side elevation windows, meaning any direct window to window overlooking (even the perception of overlooking) would not occur in this instance.

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It is recognised that noise from children in schools and outdoor play areas, particularly at break times, can give rise to significant noise levels. The applicant has provided an Environmental Noise Assessment to support the application, which deals with the internal noise levels for teaching within the classrooms and the potential for noise nuisance from external plant and equipment. The Council's Pollution Control Team has reviewed this document and confirmed that the findings are acceptable in that internal noise levels and any noise generated by external plant/equipment would not be harmful to surrounding residential properties. Conditions are added in this respect to ensure that this is the case going forward.

It is recognised that this submitted assessment however does not take into consideration noise from children in the outdoor areas of the school, including noise from cars and children arriving. Following Case Officer advice a further Noise Impact Assessment was provided, to address noise from children in outside areas at the school and noise from cars and children arriving at the school. Following review, the Council's Pollution Control Team confirmed that they are happy with the content of this assessment, and noted that whilst car park noise is likely to be generated as this will only be at the beginning and end of the school day for a relatively short period of time. As such it is concluded that any noise impact from these noise sources would not be detrimental enough to the amenity of surrounding properties to warrant refusal.

Further, in relation to the use of the external play areas it is noted that the school has been designed so that the main play areas are located away from residential properties to the west of the site. Due to this design it is therefore considered that there will be a minor increase in the existing noise climate measured on the site, with the highest predicted noise level during playtimes and lunchtimes. However, as the break and lunch times are during the middle of the day, and during weekdays only (without any significant noise generated in the late or early hours), it is again considered that any noise and disturbance generated would not be detrimental enough to the amenity of surrounding properties to warrant refusal. Mitigation is also proposed in the form of a 2.4 m high acoustic barrier installed along the northern and southern boundaries of the site (as shown on approved boundary treatment plan). The Council's Pollution Control team noted that even with acoustic barrier installed, noise from children in the outdoor areas will still be audible at nearby residential properties and that ultimately there are no suitable mitigation measures that can effectively completely remove this type of noise. To seek to further mitigate noise issues, and ensure noise and disturbance is restricted as much as possible, the use of the external areas are proposed to be managed via an Outdoor Area Management Plan (which will include details of times the areas will be used, numbers of children that will use specific areas at any one time and how the areas will be supervised). This is secured by condition set out below.

In addition to normal school use, it is noted that the new school will be used for community purposes outside of normal hours. This will be in line with the current use for the school, in that secondary facilities are currently offered to external parties from 5pm until 10pm on weekdays and from 8am until 9pm. The Council's Pollution Control Team confirmed that this is acceptable; however a condition is attached requiring that the external areas associated with the site shall not be used after 18:00pm in the interests of the amenity of neighbouring residential properties.

The Council's Pollution Control Team also raised concerns regarding noise from construction works, noise from construction vehicles and the potential for dust and mud on roads for construction activities. To reduce the effects of noise, vibration, dust and site lighting during the construction period a Construction Environmental Management Plan is therefore secured via condition.

Overall, given the minor noise level increase and the limited hours of use of the external areas (which are located largely away from residential properties) and other management and mitigation measures proposed, it is concluded that any noise and disturbance impact would not be harmful enough warrant the refusal of the application in this instance.

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To ensure any extract equipment will cause no harm to surrounding properties by virtue of odour and noise, a condition is attached requiring that detail of any equipment (including details of the flue, method of odour control, noise levels and noise attenuation measures) is provided to the Local Planning Authority for review prior to installation.

Light Pollution

An external lighting layout plan has been provided, alongside an External Lighting Assessment Report. Following review, the Council's Pollution Control team confirmed that the new lighting associated with the development would have no adverse impact on the amenities of surrounding residential properties by virtue of light pollution. A condition is however attached to ensure the light levels meet the required Obtrusive Light Limitations for Exterior Lighting Installations.

(E) HIGHWAY SAFETY, TRANSPORT AND MOVEMENT ISSUES

Policy BCS10 in Bristol Core Strategy (2011) advocates that new development should be designed and located to ensure the provision of safe streets. Policy DM23 in the Site Allocations and Development Management Policies (2014) states that development should not give rise to unacceptable traffic conditions and will be expected to provide safe and adequate access.

The Council's Transport Development Management Team (TDM) confirmed that the site is well located in a residential area, with good access to transport links. Subsequently from a principle transport/highways perspective, a school is considered acceptable in this location.

It is recognised that the presence of a new school will however generate a significant number of trips, by many modes of transport. Existing trip rates (from the existing school's hands up survey) suggest that the new schools could, once they are at capacity, generate in the region of an additional 160 car borne pupils. Some of these will be shared trips with siblings at both the proposed new primary school and adjacent Merchants Academy Secondary School. It is noted that 38% of pupils currently arrive by car to the existing primary school, which is considered to be a relatively high proportion. There is however capacity to reduce this through a strong culture of active travel, encouraged through a School Travel Plan. Nevertheless the impact will be substantially felt in the peak times, on a key public transport corridor and as such it is therefore essential that measures are put in place to reduce the impact on resultant safety concerns and congestion arising from parking. These measures are discussed below.

Firstly, the school will be required to create and implement a School Travel Plan (STP) which would need to be in place prior to the new site opening. It is recognised that a Framework Travel Plan has been prepared by PCL Transport Consultants, however it is required that the school have input into the final STP, as they will be responsible for implementing the measures. This would then be updated and monitored regularly to ensure that the measures are working. Such measures would include the provision of cycle parking, scooter parking, lockers and wet weather storage, road safety training, awareness, walking and cycling promotion, working with neighbours, and promotion of car sharing. This would also apply to staff, and include measures such as reviewing the marking system to allow staff not to feel the need to drive every day, the provision of showers and lockers to promote walking and cycling, and promotion of car sharing. The submission of this final School Travel Plan (STP) is therefore secured via condition.

Secondly, whilst the application site is accessible by public transport a new bus shelter will be required to encourage the use of the stops by additional pupils, families and staff. This will serve to reduce the number of cars on Hareclive Road and reduce delays, particularly to public transport. A financial contribution of £10,000 has been secured in this respect through a Unilateral Undertaking for

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a 3-bay reverse cantilever shelter.

The proposed school will be accessed from a new access way via Hareclive Road. There is good footway provision in the directly vicinity of the site and pedestrians are protected from passing traffic by highway verges. However, the existing pedestrian crossing island immediately in front of the new school entrance will not have adequate capacity for a school of this size, and will need to be removed to accommodate the new vehicular access. A new signalised crossing will therefore need to be installed to allow for children to cross safely with minimal disruption to the capacity of the highway. This will be at the expense of the applicant and a separate highway agreement will be required in this respect, with further details secured via condition.

Further, the current low level bollards are also insufficient to draw attention to the presence of a school and will need to be replaced by Pencil Bollards, which are used on new school schemes throughout the city. Detail of this is again secured via condition.

Also a comprehensive review of waiting restrictions will be required in order to ensure safe and effective operation of the highway network. These could be in the form of single yellow lines or double yellow lines, peak hour loading prohibition and school keep clear markings where necessary. This will require a Traffic Regulation Order and highway works, the cost of which will need to be met by the applicant. This is secured via condition and will require a separate highways agreement.

The school flashing wig wag signs will also need to be relocated in Hareclive Road and a 20mph speed limit will need to be applied to Hareclive Road on the approaches to the site. This will require a Traffic Regulation Order and highway works, the cost of which will need to be met by the applicant. This is secured via condition and will require a separate highways agreement.

The proposed vehicular access itself is satisfactory and visibility is adequate. A turning head is provided, and swept path detail has been provided with regards to a minibus and fire engine. Following consultation, the Council's Transport Development Management team raised concern regarding the swept path detail provided and the use of this turning head. Further swept path detail with regards to turning and dropping off facilities for use by minibuses into and within the site is therefore secured via condition.

It is noted that the access proposals also includes the provision of a drop off area. It is considered that dropping off facilities in general circumstances do little to reduce congestion and can create further difficulties if used by a significant number of parents, can create conflict and also act to encourage car use. It is therefore the Local Planning Authority's stance not to allow such facilities for mainstream schools. It is noted in this instance however that there is an accepted operational requirement for a drop off facility for the ASC school as these children will have complex requirements. However, the Merchants Academy School will have no vehicular access to the drop off facility. It will be essential to ensure that the access is used only by staff and those attending the Autism Hub, to reduce conflict directly outside the school. It will therefore be necessary for the school to manage this arrangement, with specific detail set out in the Travel Plan (secured via condition). A wide, segregated pedestrian access is also proposed, this is beneficial as it segregates all pedestrian access from any vehicular access.

The car parking level proposed for staff and visitors falls within the maximum standard for a school of this size. A School Travel Plan will assist in reducing the amount of on-street parking and waiting restrictions will reduce inappropriate parking. Following Case Officer advice, a sufficient amount of covered cycle parking will be provided within the curtilage of the site and this is secured by condition.

Overall, the impacts on the transport network are considered acceptable, subject to the bus stop upgrade contribution secured via a Unilateral Undertaking; a number of conditions as set out below and the agreed works to the highway being undertaken to the satisfaction of the LPA.

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Policy BCS9 in the Bristol Core Strategy (2011) states that the integrity and connectivity of the strategic green infrastructure network should be maintained, protected and enhanced. BCS21 in the same document also states that new development will be expected to deliver a safe, healthy, attractive, usable, durable and well-managed built environment comprising high quality inclusive buildings and spaces that integrate green infrastructure.

Individual green assets should be retained wherever possible and that development should incorporate new or enhanced green infrastructure of an appropriate type, standard and size. Policy DM17 in the Site Allocations and Development Management Policies (2014) states that all new development should integrate important existing trees into development proposals. Where tree loss of damage is essential to allow for appropriate development, replacement trees of an appropriate species should be provided in accordance with the tree compensation standard. Policy DM15 in the same document states that green infrastructure provision facilitates a positive effect on people's health by providing space and opportunities for sport, play, and social interaction. The provision of additional and/or improved management of existing trees will be expected as part of the landscape treatment of new development.

A number of existing trees are located within the application site. An arboricultural survey to grade the existing trees on the site and to identify their condition has been carried out. 33 existing trees have been identified for removal on site, three of which have been identified as Category A (T6, T8 and T9). Previous application reference 17/03021/F was refused by Committee in part due to the loss of these Category A trees.

It is recognised that these trees are identified for removal under the current application, with the applicant providing further justification for their loss. It is recognised that the site constraints and the requirements of the proposed school have dictated the siting of the building and this has inevitably led to the need for the removal of some trees on site. Given the site constraints and the requirements of the school, it is accepted that the proposed development could not therefore reasonably retain three the Category A trees given their position on the site.

The Council's Arboricultural Team confirmed that whilst the loss of the trees on site is regrettable they do not hold sufficiently high amenity value to warrant the refusal of the application on arboricultural grounds given their limited size and siting away from the public realm. It is recognised that the more prominent and high quality trees on site will be retained (mature Oak tree T12) which is welcomed. The applicant in addition proposes 73 replacement trees on site, which is in excess of the number required under the Bristol Tree Replacement Standard (43). Overall it is considered that the proposed landscape plan provides a significant improvement on the existing planting both in terms of number and variety of species. The positioning of new trees has been carefully considered to soften boundaries between the new building and neighbouring houses.

The application is subsequently considered acceptable on this basis, subject to a condition requiring the planting to be in accordance with the approved planting plan and another condition requiring that the Local Planning Authority shall be given not less than two weeks prior written notice by the developer of the commencement of works on the site in order that the Council may visit the site and verify in writing that the approved tree protection measures are in place around the retained trees before the work commences.

(G) DOES THE PROPOSAL RAISE ANY ECOLOGY ISSUES?

Policy DM19 in the Site Allocations and Development Management Policies (2014) states that development which would be likely to have any impact upon habitat, species or features, which contribute to nature conservation in Bristol will be expected to:

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- i. Be informed by an appropriate survey and assessment of impacts; and
- ii. Be designed and sited, in so far as practicably and viably possible, to avoid any harm to identified habitats, species and features of importance; and
- iii. Take opportunities to connect any identified on-site habitats, species or features to nearby corridors in the Wildlife Network.

The Council's Nature Conservation Officer has reviewed the application and confirmed that no endangered or priority species will be impacted upon by the proposed development. However, to ensure that no wild birds, their eggs, nests and chicks are impacted upon through the removal of small trees/shrubs a condition is attached requiring that there should be no clearance of vegetation or structures suitable for nesting birds between 1st March and 30th September (nesting season) without the prior written approval of the local planning authority. Bird boxes, reptile hibernacula and hedgehog boxes as recommended by the ecology report have been shown on the approved landscaping external furniture plan. Likewise, fruit and nut trees have been included in the planting plan. The Nature Conservation Officer has confirmed that they are happy with these proposals and these are therefore secured by relevant conditions as set out below.

(H) SUSTAINABILITY AND CLIMATE CHANGE

Current planning policy within the adopted Bristol Development Framework, Core Strategy (2011) requires new development to be designed to mitigate and adapt to climate change and meet targets to reduce carbon dioxide emissions. This should be achieved, amongst other measures, through efficient building design, the provision of on-site renewable energy generation to reduce carbon dioxide emissions by at least 20% based on the projected residual energy demand of new buildings. The approach proposed should also be supported by the provision of a sustainability statement and an energy strategy. For major development and development for health or education uses the Sustainability Statement should include a BREEAM assessment.

Following consultation the Council's Sustainable Cities Team confirmed that the application was acceptable and that appropriate measures had been undertaken to mitigate and adapt to climate change and meet targets to reduce carbon dioxide emissions. Solar panels are proposed on the roof of the building, which will achieve the required saving of 20% and the details of these are conditioned accordingly.

It is noted that there is no district heating network in the vicinity of the development and therefore the non-inclusion connection measures is accepted. The pre-assessment estimate of BREEAM Very Good is noted as are the reasons why 'Excellent' is regarded as unachievable. BREEAM Excellent is deemed neither commercially or operationally viable in this instance. By example, to achieve BREEAM Excellent one of the key risk and commercial related credits is Ene 01 where a mandatory 5 credits are required. The current design achieves zero credits for Ene 01. To achieve the minimum 5 credits would require alternative ventilation systems provided to the classrooms which incorporate heat recovery. Such systems would be contrary to the Education and Skills Funding Agency's (ESFA's) Facilities Output Specification and would conflict with the design philosophy for schools developed by the ESFA over recent years. The current design is energy efficient and meets the ESFA requirements for energy efficiency and provides draught-free comfort via room-type mixing units. These ESFA compliant energy efficient room mixing units which control comfort and air quality have an adverse effect on the Ene 01 score, hence zero credits. The current scheme also complies with Part L, with a good pass margin >10%. This specification is written towards a desire for buildings to be BREEAM Very Good in design, delivered through its environmental/sustainability strategy and its holistic approach to other design elements. The ESFA are happy with the proposed design and that it meets the requirements of their specification. A condition is attached requiring that once the building is complete (i.e. prior to occupation) a further report/statement is provided to the LPA confirming the 'Very Good' rating (or equivalent) has been achieved.

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The application is subsequently considered acceptable on this basis, subject to conditions.

(I) AIR QUALITY

Policy BCS23 in the Core Strategy (2011) states that development should be sited and designed in a way as to avoid adversely impacting upon the amenity of the surrounding area by reason of fumes, dust, noise, vibration, smell, light and other forms of pollution. Policy DM35 in the Site Allocations and Development Management Policies (2014) states that any scheme which will have an unacceptable impact on environmental amenity by reason of noise or odour will be expected to provide an appropriate scheme of mitigation.

It is recognised that the new car parking area will be sited in relatively close proximity to neighbouring residential properties. Following consultation however, the Council's Air Quality Team confirmed that given the limited number of proposed spaces any vehicle fumes would not be harmful to surrounding residents, nor would the scale of development result in any wider air quality issues. The application is subsequently considered acceptable on this basis.

(J) FLOOD RISK

Bristol Core Strategy (2011) Policy BCS16 states that all development will also be expected to incorporate water management measures to reduce surface water run-off and ensure that it does not increase flood risks elsewhere. This should include the use of sustainable drainage systems (SUDS).

The applicant has provided a Drainage Strategy and SUDS Statement to support the application. Following consultation, the Council's Flood Risk Team confirmed that the detail contained within this report is acceptable to satisfy concerns whether the development would result in any harmful levels of surface runoff. A condition is however attached to ensure a detailed drainage design formed in accordance with the approved strategy is submitted to and approved by the Council prior to the commencement of development.

(K) DOES THE PROPOSAL GIVE RISE TO ANY CONTAMINATION ISSUES?

Policy DM34 in the Site Allocations and Development Management Policies (2014) states that new development should demonstrate that:

- i. Any existing contamination of the land will be addressed by appropriate mitigation measures to ensure that the site is suitable for the proposed use and that there is no unacceptable risk of pollution within the site or in the surrounding area; and
- ii. The proposed development will not cause the land to become contaminated, to the detriment of future use

Following consultation, the Council's Contaminated Land team confirmed that the Ground Investigation Report submitted to support the application adequately assesses risks from contamination. Further assessment of contamination is therefore not required, however given previous experience of school sites of this age with encountering unexpected contaminants in the site soils, a condition is attached requiring that the applicant reports any unexpected contamination to the Council immediately if encountered.

(L) DOES THE PROPOSAL RAISE ANY CRIME OR SECURITY ISSUES?

Following consultation, the Avon and Somerset Polices Crime Prevention Design Adviser raised no objections to the application.

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Policy BCS11 of the Core Strategy (2011) requires that planning obligations should be secured through the planning process in order to offset the impact of the proposed development on the local infrastructure. The obligations and how they are secured in this case are as follows:

Public Art

See Key Issue C above. Secured via conditions.

Landscape/Public Realm Scheme/Trees

See Key Issues C, F and G above. Secured via conditions.

Travel Plan

See Key Issue E above. Secured via condition.

Highway Works

See Key Issue E above. Secured via Conditions and a separate financial contribution of £10,000 for a replacement bus shelter secured by Unilateral Undertaking.

CONCLUSION

In accordance with local and national planning policy, the proposed development fulfils an important educational need and will ensure that a sufficient choice of school places is available to meet the needs of existing and new communities.

It is recognised that the development is very similar to that proposed under previous application reference 17/03021/F, which was refused by Committee due to the perceived harmful amenity impact (overlooking, overshadowing and overbearing) as well as the loss of 3.no category A trees. Changes have been made to the proposed scheme and further information has been provided under the current application the in an attempt to address the previous refusal reason.

Following a robust assessment taking into consideration the revised plans and additional detail and giving great weight to the need to create, expand or alter schools through the preparation of plans and decisions on applications as identified within the NPPF, the Local Planning Authority is satisfied that the proposal is acceptable. Whilst the siting and scale of the development remains far from ideal, following amendments secured as part of the previous application and following the submission of revised plans and further detail under the current application it is considered that any amenity impact (overbearing, overshadowing, overlooking) would on balance not warrant the refusal of a scheme that will deliver significantly important school places.

Whilst the loss of the Category A trees on site is regrettable it is accepted that given the site constraints and the requirements of the school, the proposed development could not reasonably retain three Category A trees given their position on the site. The Council's Arboricultural Officer has advised that they do not hold sufficiently high amenity value to warrant the refusal of the application on arboricultural grounds given their limited size and siting away from the public realm. It is recognised that the more prominent and high quality trees on site will be retained (mature Oak tree T12) which is welcomed. The applicant in addition proposes 73 replacement trees on site, which is in excess of the number required under the Bristol Tree Replacement Standard (43). Overall it is considered that the proposed landscape plan provides a significant improvement on the existing planting both in terms of number and variety of species, and is supported.

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The proposal is considered acceptable in relation to design, highways/transport, ecology, flood risk, land contamination, air quality and sustainability; subject to conditions.

As such the approval of the application is recommended to Members, subject to conditions.

COMMUNITY INFRASTRUCTURE LEVY

How much Community Infrastructure Levy (CIL) will this development be required to pay?

The Bristol Community Infrastructure Levy (CIL) Charging Schedule sets out that non-residential institutions (which includes schools such as the one proposed here) are £0 rated, as such no CIL receipts are anticipated from this development.

RECOMMENDED GRANTED subject to condition(s)**Time limit for commencement of development**

1. Full Planning Permission

The development hereby permitted shall begin before the expiration of three years from the date of this permission.

Reason: As required by Section 91 of the Town and Country Planning Act 1990, as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

Pre commencement condition(s)

2. Construction management plan

No development shall take place including any works of demolition (with the exception of erection of the single storey environmental classroom and enabling works) until a construction management plan or construction method statement has been submitted to and been approved in writing by the Local Planning Authority. The approved plan/statement shall be adhered to throughout the construction period. The statement shall provide for:

- Parking of vehicles of site operatives and visitors.
- Routes for construction traffic
- Method of preventing mud being carried onto the highway.
- Pedestrian and cyclist protection.
- Proposed temporary traffic arrangements including hoardings and/or footway closures.
- Arrangements for turning vehicles.
- Arrangements to receive abnormal loads or unusually large vehicles.
- How the delivery of construction materials and the collection of waste will be managed.
- Where construction materials and waste will be stored.
- Methods of communicating the Construction Management Plan to staff, visitors and neighbouring residents and businesses.

Reason: In the interests of safe operation of the highway in the lead into development both during the demolition and construction phase of the development.

3. Site Specific Construction Environmental Management Plan

No development shall take place including any works of demolition (with the exception of erection of the single storey environmental classroom and enabling works) until a site specific Construction Environmental Management Plan has been submitted to and been approved in

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writing by the Council. The plan must demonstrate the adoption and use of the best practicable means to reduce the effects of noise, vibration, dust and site lighting on the surrounding area and all surrounding premises and infrastructure.

Reason: In the interests of the amenities of surrounding occupiers during the construction of the development.

4. Highway Works

Prior to commencement of the construction of the main school building hereby permitted general arrangement plan(s) indicating the following works to the highway shall be submitted and approved in writing by the Local Planning Authority

- Removal of traffic island and making good of the highway
- Installation of signalised crossing
- Scheme of waiting restrictions in the vicinity of the site
- Extension of 20mph zone and associated signing
- Relocation of school flashing warning sign
- Associated ancillary works including without limitation dropped kerbs, pencil bollards, footway crossovers, tactile paving, street lighting, pavement reinstatement, drainage, reinstatement of redundant accessways, lowering of any services and return to store of recyclable materials

The drawings must indicate proposals for:

- Threshold levels of the finished highway and building levels
- Alterations to waiting restrictions or other Traffic Regulation Orders to enable the works
- Locations of lighting, signing, street furniture, street trees and pits
- Structures on or adjacent to the highway
- Extents of any stopping up or dedication of new highway

No building or use hereby permitted shall be occupied or the use commenced until these works are completed to the satisfaction of the Local Highway Authority and as approved in writing by the Local Planning Authority.

Reason: In the interests of public safety and to ensure that all road works associated with the proposed development are planned and approved in good time to include any statutory processes, are undertaken to a standard approved by the Local Planning Authority, and are completed before occupation.

5. Protection of Retained Trees During the Construction Period

No demolition or construction work of any kind shall begin on the site until the approved fences and protection has been erected around the retained trees in the position and to the specification detailed in the Arboricultural Impact Assessment and Arboricultural Method Statement prepared by Advanced Arboriculture, referenced TH/X1600/0617 and dated June 2017 and as shown on the approved Tree Protection Plan: Overview (TH/X1600/0617 1); Tree Protection Plan: North (TH/X1600/0617 1); Tree Protection Plan: Central (TH/X1600/0617 1) and Tree Protection Plan: South (TH/X1600/0617 1); Arboricultural Method Statement Plan (TH/X1600/0617 1) and Braced Heras Fencing (AGS101 1).

The Local Planning Authority shall be given not less than two weeks prior written notice of the completed installation of the protective fencing by the developer prior to the commencement of works on the site in order that the Local Planning Authority may verify in writing that the approved tree protection measures are in place when the work commences.

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The approved fences and ground protection shall be in place before any equipment, machinery or materials are brought on to the site for the purposes of demolishing or development and shall be maintained until all equipment, machinery and surplus materials have been removed from the site.

Within the fenced area(s) there shall be no scaffolding, no stockpiling of any materials or soil, no machinery or other equipment parked or operated, no traffic over the root system, no changes to the soil level, no excavation of trenches, no site huts, no fires lit, no dumping of toxic chemicals and no retained trees shall be used for winching purposes. If any retained tree is removed, uprooted or destroyed or dies, another tree shall be planted at the same place and that tree shall be of such size and species, and shall be planted at such time, as may be specified in writing by the Local Planning Authority. Under no circumstances should the tree protection be moved during the period of the development and until all works are completed and all materials and machinery are removed. Landscaping works within protected areas is to be agreed with the Local Planning Authority and carried out when all other construction and landscaping works are complete.

Reason: To protect the retained trees from damage during construction and in recognition of the contribution which the retained trees give and will continue to give to the amenity of the area.

6. Sustainable Drainage System (SuDS)

The development hereby approved (excluding site preparation/enabling works and the erection of the single storey environmental classroom) shall not commence until a Sustainable Drainage Strategy and associated detailed design, management and maintenance plan of surface water drainage for the site using SuDS methods has been submitted to and approved in writing by the Local Planning Authority in accordance with the approved Preliminary Drainage Strategy and SuDS Statement referenced 2160451 P2 and dated May 2018. The approved drainage system shall be implemented in accordance with the approved Sustainable Drainage Strategy prior to the use of the building first commencing and then maintained thereafter for the lifetime of the development.

Reason: To prevent the increased risk of flooding by ensuring the provision of a satisfactory means of surface water disposal is incorporated into the design and the build and that the principles of sustainable drainage are incorporated into this proposal and maintained for the lifetime of the proposal.

7. Vegetation Clearance

No clearance of vegetation or structures suitable for nesting birds, shall take place between 1st March and 30th September inclusive in any year without the prior written approval of the Local Planning Authority. The authority will require evidence provided by a suitably qualified ecologist that no breeding birds would be adversely affected including by disturbance before giving any approval under this condition. Where checks for nesting birds by a qualified ecological consultant are required they shall be undertaken no more than 48 hours prior to the removal of vegetation or the demolition of, or works to buildings.

Reason: To ensure that wild birds, building or using their nests are protected.

8. Public Art

The development shall be undertaken in accordance with the concept Art Plan prepared by Pony and dated August 2017 and prior to the construction of the main school building hereby

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permitted (unless an alternative timescale for the submission of these details supported by a formal phasing plan is submitted to and approved in writing by the Local Planning Authority) detailed drawings (plans and elevations) for the public artworks for the school (entrance, entrance landscape, and rear landscape) shall be submitted to and approved in writing by the Local Planning Authority in the form of an Art Plan. The detailed plans will be accompanied by a timetable for the installation of the artworks across the site in line with the development and landscape build programme in which they shall be integrated, together with details of how the school will maintain the work in the form of a maintenance and care manual.

Reason: To ensure that public art is integrated into the design of the development.

9. Details of Extraction/Ventilation System

No extraction/ventilation systems for the extraction and dispersal of cooking odours shall be installed until details of the flue, method of odour control, noise levels and noise attenuation measures has been submitted to and approved in writing by the Council. The approved scheme shall be implemented prior to the first commencement of the use and and then be permanently maintained thereafter.

Reason: In order to safeguard the amenities of nearby occupiers.

10. Material samples

Prior to the commencement of the relevant works sample panels of the proposed external render, brick and timber cladding demonstrating the colour, finish, coursing, jointing and pointing to the masonry are to be erected on site and approved in writing by the Local Planning Authority, unless otherwise agreed in writing by the Local Planning Authority. The development shall be completed in accordance with the approved details, unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure that the external appearance of the building is satisfactory.

11. Prior to the commencement of relevant works drawings to a minimum 1:10 scale (also indicating materials, treatments, and finishes) of the following items shall be submitted to and approved in writing by the Local Planning Authority unless otherwise agreed in writing by the Local Planning Authority:

- (a) All new windows and doors (including sectional profiles)
- (b) Rooftop railings

The detail thereby approved shall be carried out in accordance with that approval.

Reason: To ensure that the external appearance of the building is satisfactory

12. Further information - internal vehicular access and parking

Prior to the commencement of relevant works details of turning and dropping off facilities for use by minibuses into and within the site shall be submitted to and approved in writing by the Local Planning Authority. No building or use hereby permitted shall be occupied or use commenced until the facilities for loading, unloading, circulation, parking and manoeuvring have been completed in accordance with the approved plans. Thereafter, these areas shall be kept available for these uses.

Reason: To ensure that there are adequate servicing and parking facilities within the site in the interests of highway safety.

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13. Imported Soils Verification

For each phase any topsoil (natural or manufactured) or subsoil to be imported shall be assessed for chemical or other potential contaminants in accordance with a scheme of investigation which shall be submitted to and approved in writing by the Local Planning Authority in advance of its importation. Only material approved by the Local Planning Authority shall be imported. All measures specified in the approved scheme shall be undertaken in accordance with Pollution Control's Imported Materials Guidance Notes. Subject to approval of the above, sampling of the material received at the development site to verify that the imported soil is free from contamination shall be undertaken in accordance with a scheme and timescale to be agreed in writing by the Local Planning Authority.

Reason: To ensure that the safety of future occupiers is not prejudiced.

Pre occupation condition(s)

14. Land affected by contamination - Reporting of Unexpected Contamination

In the event that contamination is found at any time that had not previously been identified when carrying out the approved development, it must be reported immediately to the Local Planning Authority. An investigation and risk assessment must be undertaken in accordance with DEFRA and the Environment Agency's 'Model Procedures for the Management of Land Contamination, CLR 11', and where remediation is necessary a remediation scheme must be prepared which ensures the site will not qualify as contaminated land under Part 2A of the Environmental Protection Act 1990 in relation to the intended use of the land after remediation.

Following completion of measures identified in the approved remediation scheme a verification report must be prepared, which is subject to the approval in writing of the Local Planning Authority. The Local Planning Authority must be given two weeks written notification of commencement of the remediation scheme works.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

15. Solar panels

No building or use hereby permitted shall be occupied or the use commenced until details including the final roof layout, visual appearance, angle of installation, method of fixing, and technical specifications (including the output to demonstrate compliance with the approved Sustainability Report) of the solar panels have been submitted to and approved in writing by the Local Planning Authority. The solar panels shall be installed in accordance with the approved details and made fully operational prior to the first occupation of school hereby permitted. The solar panels shall be maintained in situ in accordance with the approved details and as fully operational at all times thereafter.

Reason: To ensure that the development would meet sustainability and climate change policy objectives.

Development Control Committee A – 5 September 2018

Application No. 18/03233/F : Merchants Academy Gatehouse Avenue Bristol BS13 9AJ

16. Outdoor Area Management Plan

No building or use hereby permitted shall be occupied or the use commenced until there has been submitted to and approved in writing by the Local Planning Authority an Outdoor Area Management Plan for the development hereby approved, setting out details of times the areas will be used, numbers of children that will use specific areas at any one time and how the areas will be supervised. The approved outdoor management plan shall be complied with throughout the duration of the use.

Reason: In order to safeguard the amenities of nearby occupiers.

17. Travel Plans - Not submitted

No building or use hereby permitted shall be occupied or the use commenced until a Travel Plan comprising immediate, continuing and long-term measures to promote and encourage alternatives to single-occupancy car use has been prepared, submitted to and been approved in writing by the Local Planning Authority. The approved Travel Plan shall then be implemented, monitored and reviewed in accordance with the agreed travel Plan Targets to the satisfaction of the council.

This will also include arrangements for the management of the parking and dropping off facility to ensure this is only used by/for pupils of Venturers Academy

The approved Travel Plan shall then be implemented, monitored and reviewed in accordance with the agreed travel Plan Targets to the satisfaction of the council.

Reason: In order to deliver sustainable transport objectives including a reduction in single occupancy car journeys and the increased use of public transport, walking & cycling.

18. BREEAM

No building shall be occupied or the use commenced until a report/statement from the Technical Advisor certifying that the building achieves BREEAM (or any such equivalent national measure of sustainable building which replaces that scheme) equivalent rating 'Very Good' has been achieved for this development has been submitted to and approved by the Local Planning Authority, unless otherwise agreed in writing with the Local Planning Authority.

Reason: To ensure that the development achieves BREEAM rating level 'Very Good' (or any such equivalent national measure of sustainability for building design which replaces that scheme) and to ensure that the development contributes to mitigating and adapting to climate change and to meeting targets to reduce carbon dioxide emissions.

19. Completion and Maintenance of Car/Vehicle Parking - Shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the car/vehicle parking area shown on the approved plans has been completed, and thereafter, the area shall be kept free of obstruction and available for the parking of vehicles associated with the development

Reason: To ensure that there are adequate parking facilities to serve the development.

Development Control Committee A – 5 September 2018

Application No. 18/03233/F : Merchants Academy Gatehouse Avenue Bristol BS13 9AJ

20. Completion of Pedestrians/Cyclists Access - Shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the means of access for pedestrians and/or cyclists have been constructed in accordance with the approved plans and shall thereafter be retained for access purposes only.

Reason: In the interests of highway safety.

21. Completion and Maintenance of Cycle Provision - Shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the cycle parking provision shown on the approved plans has been completed, and thereafter, be kept free of obstruction and available for the parking of cycles only.

Reason: To ensure the provision and availability of adequate cycle parking.

22. Implementation/Installation of Refuse Storage and Recycling Facilities - Shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the refuse store, and area/facilities allocated for storing of recyclable materials, as shown on the approved plans have been completed in accordance with the approved plans. Thereafter, all refuse and recyclable materials associated with the development shall either be stored within the dedicated store/area, as shown on the approved plans, or internally within the building(s) that form part of the application site. No refuse or recycling material shall be stored or placed for collection on the public highway or pavement, except on the day of collection.

Reason: To safeguard the amenity of the occupiers of adjoining premises, protect the general environment, and prevent obstruction to pedestrian movement, and to ensure that there are adequate facilities for the storage and recycling of recoverable materials.

23. Completion of Vehicular Access - Shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the means of vehicular access has been constructed and completed in accordance with the approved plans and the said means of vehicular access shall thereafter be retained for access purposes only.

Reason: In the interests of highway safety.

24. Installation of acoustic fence

No building or use hereby permitted shall be occupied or the use commenced until the timber acoustic fence has been installed in the position and to the specification as shown on approved plans 4238-HED-00-DR-L-2001 P7 and MAB-HED-L-XX-DR-5044 and shall thereafter be maintained as such in perpetuity unless otherwise agreed in writing by the Local Planning Authority.

Reason: In order to safeguard the amenities of nearby occupiers.

25. Landscape

The new planting, landscaping and ecology measures shall be completed to the specification as shown on approved plans MAB-HED-L-XX-DR-2008 P5; 4238-HED-00-DR-L-2004 P3 and

Development Control Committee A – 5 September 2018

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4238-HED-00-DR-L-2000 P4 in the first planting season following completion of development unless otherwise agreed in writing by the Local Planning Authority.

Reason: In the interests of ecology and the visual amenity of the area.

Post occupation management

26. Restriction of noise from plant and equipment

The rating level of any noise generated by plant & equipment as part of the development shall be at least 5 dB below the background level as determined by BS4142: 2014 Methods for rating and assessing industrial and commercial sound.

Reason: To safeguard the amenity of nearby premises and the area generally.

27. External lighting

Artificial lighting to the development must meet the Obtrusive Light Limitations for Exterior Lighting Installations in table 2 of the Institute of Light Engineers Guidance Notes for the Reduction of Obtrusive Lighting, GN01:2011.

Reason: To safeguard the amenity of nearby premises and the area generally.

28. Use of Refuse and Recycling facilities

Activities relating to the collection of refuse and recyclables and the tipping of empty bottles into external receptacles shall only take place between 08.00 and 20.00 Monday to Saturday and not at all on Sundays or Bank Holidays

Reason: To safeguard the amenity of nearby premises and the area generally.

29. Deliveries

Activities relating to deliveries shall only take place between 08.00 and 20.00 Monday to Saturday and not at all on Sundays or Bank Holidays.

Reason: To safeguard the amenity of nearby premises and the area generally.

30. Hours of Operation - External Areas

The use of any of the external areas of the school hereby approved including associated access and parking areas shall not take place outside the hours of 08:00 to 18:00 Monday to Friday; 09:00 to 18:00 on Saturdays and not at all on Sundays and Bank Holidays.

Reason: To safeguard the amenity of nearby premises from undue noise and disturbance.

31. Hours of Operation - Community Use

The use of the building for community use purposes shall not take place outside the hours of 08.00 - 22.00 Monday to Friday and 08.00 - 21.00 on Saturday and Sunday.

Reason: To safeguard the amenity of nearby premises from undue noise and disturbance.

Development Control Committee A – 5 September 2018**Application No. 18/03233/F : Merchants Academy Gatehouse Avenue Bristol BS13 9AJ**

32. Obscured glazed windows

The proposed windows shall be glazed with obscure glass as shown on the approved plans TP(11)001 REV 9 and TP(11)002 REV 9 and shall be maintained thereafter as obscure glazed as shown on approved plans unless agreed in writing by the Local Planning Authority.

Reason: To safeguard the amenities of the adjoining premises from overlooking and loss of privacy.

33. Works to trees

The development permitted by this planning permission shall be carried out in complete accordance with the Arboricultural Impact Assessment and Arboricultural Method Statement prepared by Advanced Arboriculture, referenced TH/X1600/0617 and dated June 2017 and the Landscape Management Plan prepared by Hyland Edgar Driver and dated 9 August 2017, unless otherwise agreed in writing by the Local Planning Authority.

Reason: To protect the retained trees from damage during construction and in recognition of the contribution which the retained trees give and will continue to give to the amenity of the area.

34. Sustainability statement

The development permitted by this planning permission shall be carried out in complete accordance with the Sustainability Report Volume 1.3 prepared by Silcock Dawson & Partners and dated June 2018, unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure that the development makes sufficient contribution towards mitigating and adapting to climate change.

35. Roof Access

Access to all roof areas shall be for the purposes of maintenance and emergency access only and not be used as external amenity space or roof terraces.

Reason: To safeguard the amenities of the adjoining premises from overlooking and loss of privacy.

List of approved plans

36. List of approved plans and drawings

The development shall conform in all aspects with the plans and details shown in the application as listed below, unless variations are agreed by the Local Planning Authority in order to discharge other conditions attached to this decision.

4184-500 REV A Fire appliance VSP, received 12 June 2018

4184-501 REV B Mini bus, received 12 June 2018

4238-HED-00-DR-L-2000 P4 Landscape layout, received 12 June 2018

4238-HED-00-DR-L-2001 P7 Boundary treatment, received 12 June 2018

4238-HED-00-DR-L-2002 P5 Hard surfacing, received 12 June 2018

4238-HED-00-DR-L-2003 P3 External furniture, received 12 June 2018

4238-HED-00-DR-L-2004 P3 Planting plan, received 12 June 2018

4238-HED-00-DR-L-2005 P4 Landscape levels, received 12 June 2018

4238-HED-00-DR-L-2006 Planting palette, received 12 June 2018

Development Control Committee A – 5 September 2018**Application No. 18/03233/F : Merchants Academy Gatehouse Avenue Bristol BS13 9AJ**

4238-HED-00-DR-L-2007 P3 Landscape sections, received 12 June 2018
 MAB-HED-L-X-DR-5017 REV B Environmental classroom, received 12 June 2018
 MAB-HED-L-XX-DR-2008 P5 Tree removal and retention plan, received 12 June 2018
 MAB-HED-L-XX-DR-5016 P1 Walled garden, received 12 June 2018
 MAB-HE MAB-HED-L-XX-DR-5023 Rev B Environmental classroom elevations, received 12 June 2018
 MAB-HED-L-XX-DR-5044 Timber acoustic fencing, received 12 June 2018
 MAB-HED-L-XX-DR-5051 Tree and hedge planting, received 12 June 2018
 MAB-HED-L-XX-DR-5062 Thermoplastic line marking, received 12 June 2018
 MAB-HED-L-XX-DR-5063 REV 1 Front entrance details, received 12 June 2018
 MAB-HED-L-XX-DR-5064 REV 1 Venturers play area detail, received 12 June 2018
 MAB-HED-L-XX-DR-5070 REV 1 Reception feature fence, received 12 June 2018
 MAB-HED-L-XX-DR-5071 Front railings elevations, received 12 June 2018
 MAB-HED-L-XX-DR-5072 P1 Cycle provision, received 12 June 2018
 TP(00)001 REV 1 Site location plan, received 12 June 2018
 TP(00)002 REV 1 Site constraints, received 12 June 2018
 TP(00)004 REV 7 Site plan, received 12 June 2018
 TP(00)005 REV 6 Cross section and street elevation, received 12 June 2018
 TP(00)006 REV 5 Cross sections 2, received 12 June 2018
 TP(00)007 REV 4 Annual shadow plan, received 12 June 2018
 TP(00)008 REV 5 Detailed shadow analysis, received 12 June 2018
 TP(00)009 REV 3 Eyesight levels plan, received 12 June 2018
 TP(10)001 REV 6 Ground floor plan, received 12 June 2018
 TP(10)002 REV 8 First floor and roof plan, received 12 June 2018
 TP(11)001 REV 9 Elevations, received 12 June 2018
 TP(11)002 REV 9 Key elevations, received 12 June 2018
 Sustainability Report Volume 1.3 prepared by Silcock Dawson & Partners and dated June 2018, received 12 June 2018
 Arboricultural Impact Assessment and Arboricultural Method Statement prepared by Advanced Arboriculture, referenced TH/X1600/0617 and dated June 2017, received 12 June 2018
 Landscape Management Plan prepared by Hyland Edgar Driver and dated 9 August 2017, received 12 June 2018

Reason: For the avoidance of doubt.

Advices

- 1 Works on the public highway: The development hereby approved includes the carrying out of work on the public highway. You are advised that before undertaking work on the highway you must enter into a formal agreement with the council which would specify the works and the terms and conditions under which they are to be carried out. You should contact You should contact TDM - Strategic City Transport (100TS), Bristol City Council, PO Box 3176, Bristol, BS3 9FS, telephone 0117 903 6846 or email TransportDM@bristol.gov.uk, allowing sufficient time for the preparation and signing of the agreement. You will be required to pay fees to cover the councils cost's in undertaking the following actions:
 - 1) Drafting the agreement
 - 2) A monitoring fee equivalent to 15% of the planning application fee
 - 3) Approving the highway details
 - 4) Inspecting the highway works.

- 2 Planning permission is not permission to work in the highway. A Highway Agreement under Section 278 of the Highways Act 1980 must be completed, the bond secured and the City Council's technical approval and inspection fees paid before any drawings are considered and approved and formal technical approval is necessary prior to any works being permitted

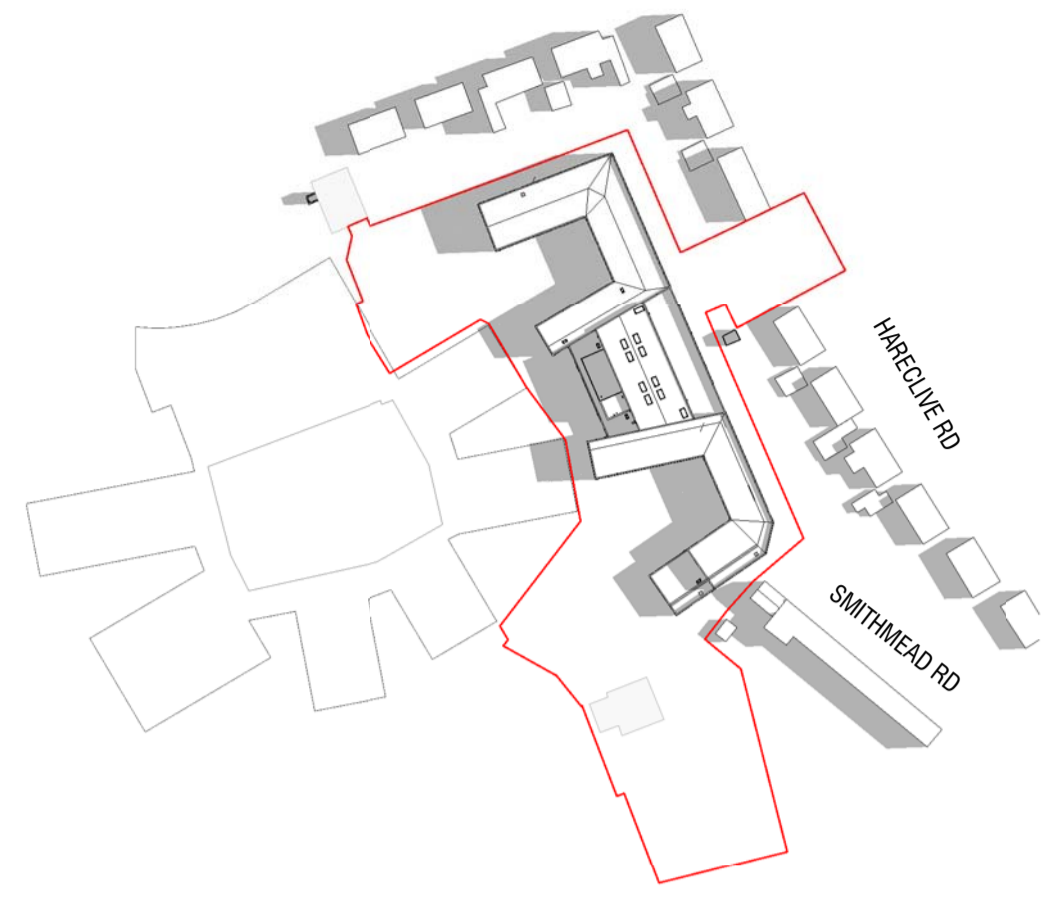
Development Control Committee A – 5 September 2018**Application No. 18/03233/F : Merchants Academy Gatehouse Avenue Bristol BS13 9AJ**

- 3 Traffic Regulation Order (TRO): In order to comply with the requirements of condition 3 you are advised that the implementation of a TRO is required. The TRO process is a lengthy legal process involving statutory public consultation and you should allow an average of 6 months from instruction to implementation. You are advised that the TRO process cannot commence until payment of the TRO fees are received. Telephone 0117 9036846 to start the TRO process.
- 4 The development hereby approved is likely to impact on the highway network during its construction. The applicant is required to contact Highway Network Management to discuss any temporary traffic management measures required, such as footway, Public Right of Way or carriageway closures, or temporary parking restrictions. Please call 0117 9036852 or email traffic@bristol.gov.uk a minimum of eight weeks prior to any activity on site to enable Temporary Traffic Regulation Orders to be prepared and a programme of Temporary Traffic Management measures to be agreed.
- 5 Construction site noise: Due to the proximity of existing noise sensitive development and the potential for disturbance arising from contractors' operations, the developers' attention is drawn to Section 60 and 61 of the Control of Pollution Act 1974, to BS 5528: Parts 1 and 2: 2009 Noise and Vibration Control on Construction and Open Sites code of practice for basic information and procedures for noise and vibration control" and the code of practice adopted by Bristol City Council with regard to "Construction Noise Control". Information in this respect can be obtained from Pollution Control, City Hall, Bristol City Council, PO Box 3176, Bristol BS3 9FS.
- 6 Nesting birds: Anyone who takes, damages or destroys the nest of any wild bird whilst that nest is in use or being built is guilty of an offence under the Wildlife and Countryside Act 1981 and prior to commencing work you should ensure that no nesting birds will be affected.
- 7 All species of bats and their roosts are legally protected. If bats are encountered all demolition or construction work should cease and the Bat Conservation Trust (Tel 0845 1300 228) should be consulted for advice.
- 8 You are reminded of the need to obtain separate consent under the Town and Country Planning (Control of Advertisements) Regulations 1992 for any advertisements requiring express consent which you may wish to display on these premises.

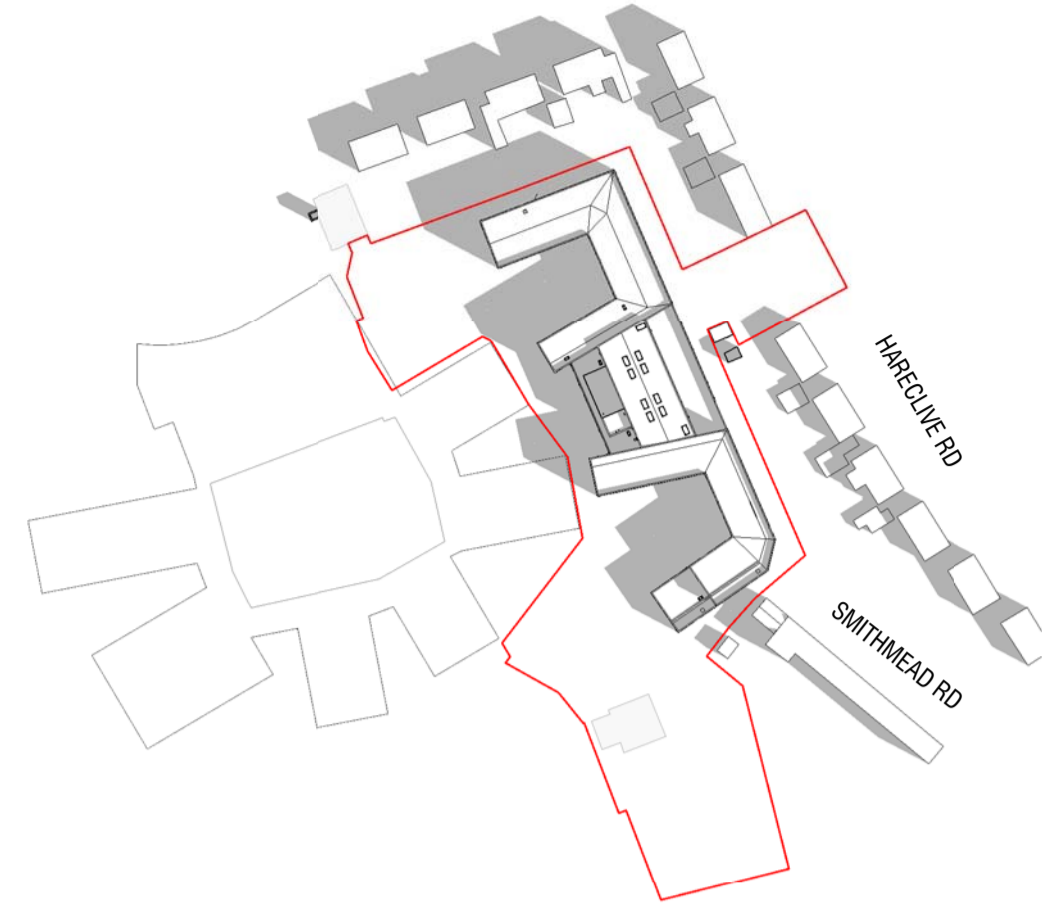
Supporting Documents

1. Merchants Academy, Gatehouse Avenue

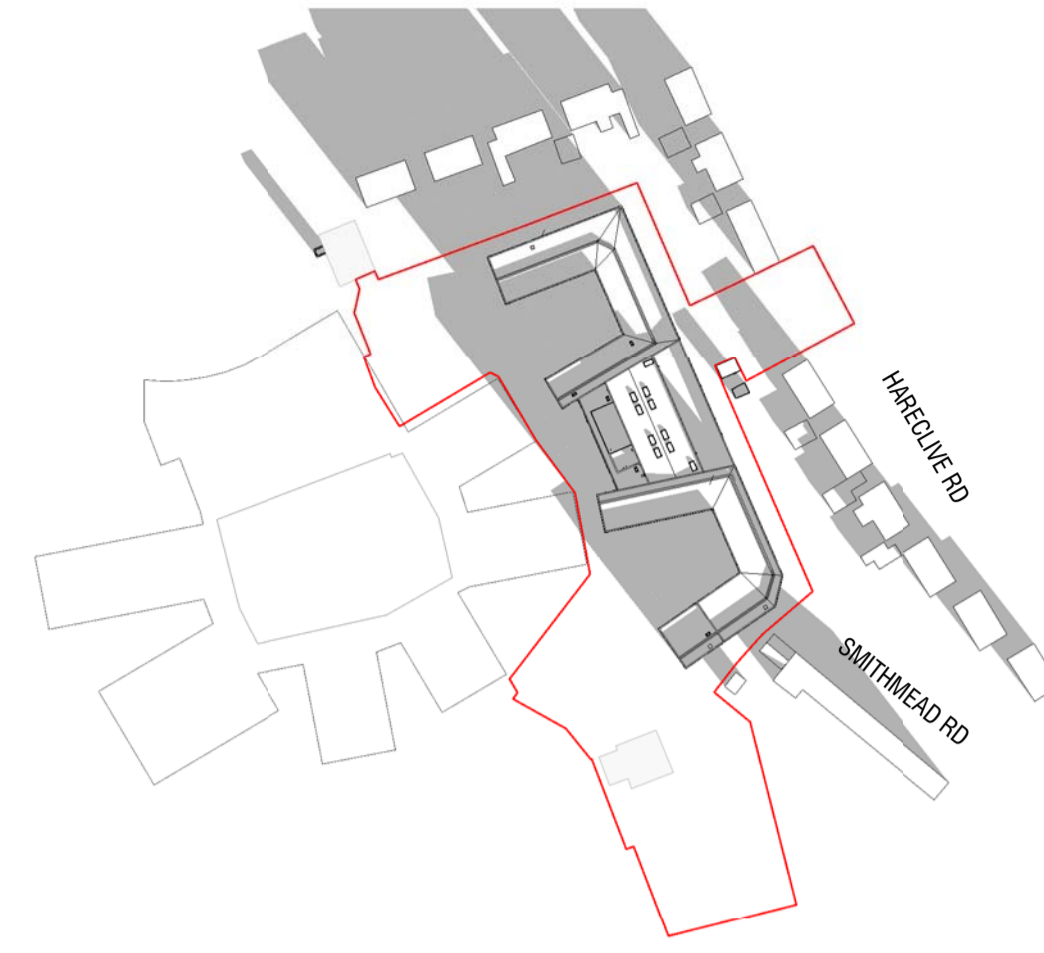
1. Annual shadow plan
2. Eyesight levels plan
3. Proposed elevations
4. Proposed first floor & roof plan
5. Proposed ground floor plan
6. Site cross sections 2
7. Site cross sections & street elevations
8. Tree removal & retention plan



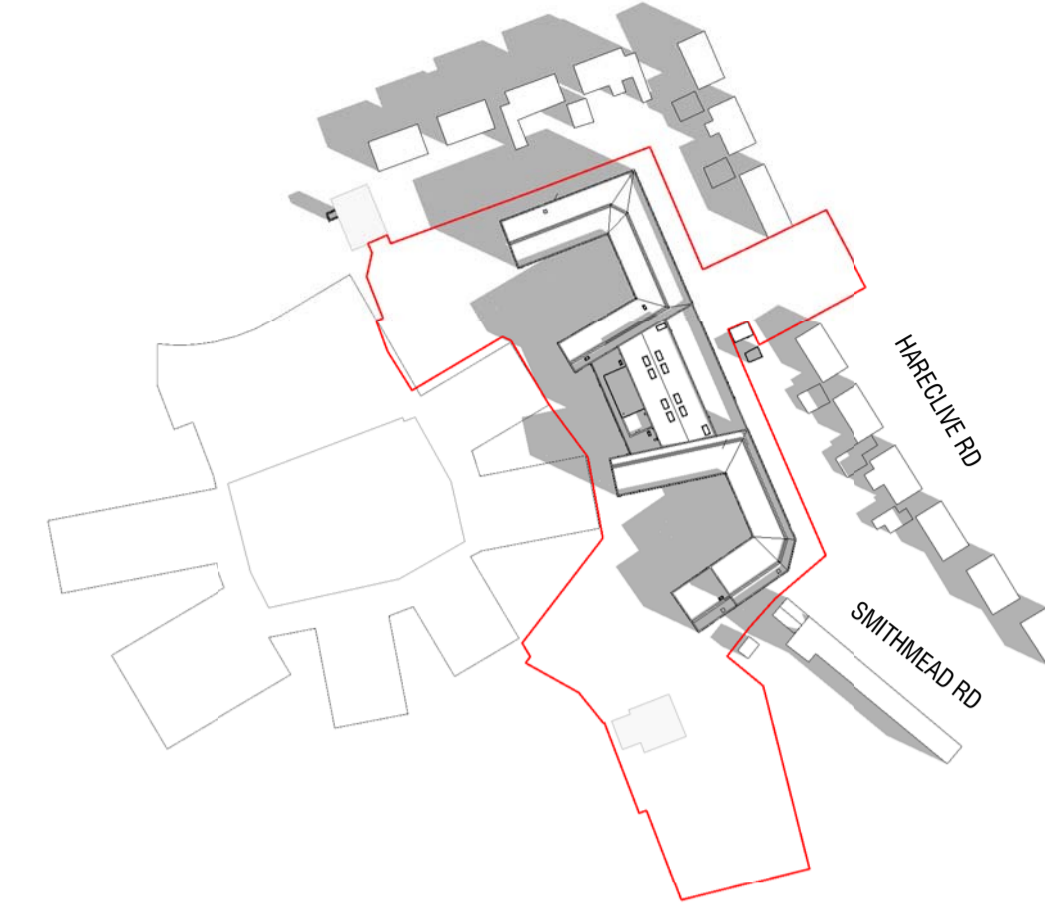
1 June 21st 8am
 TP(00)007 1 : 2000



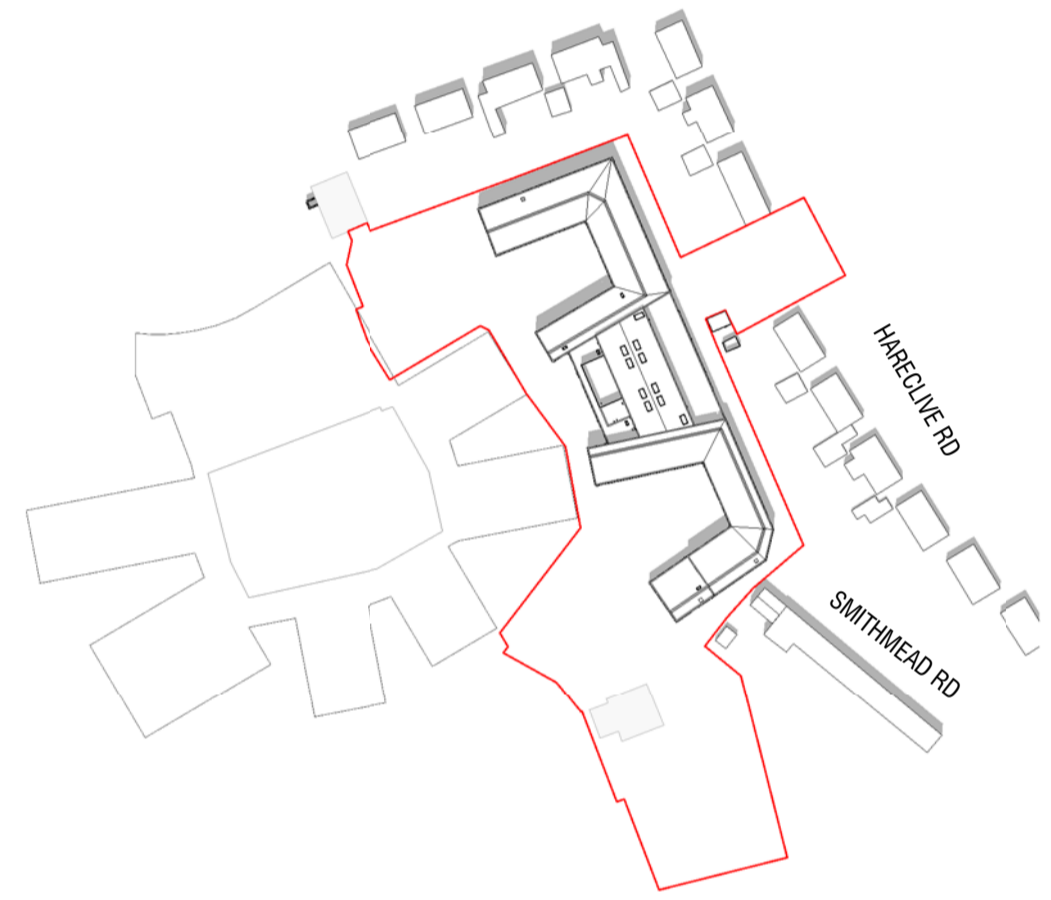
4 Sep 21st 9am
 TP(00)007 1 : 2000



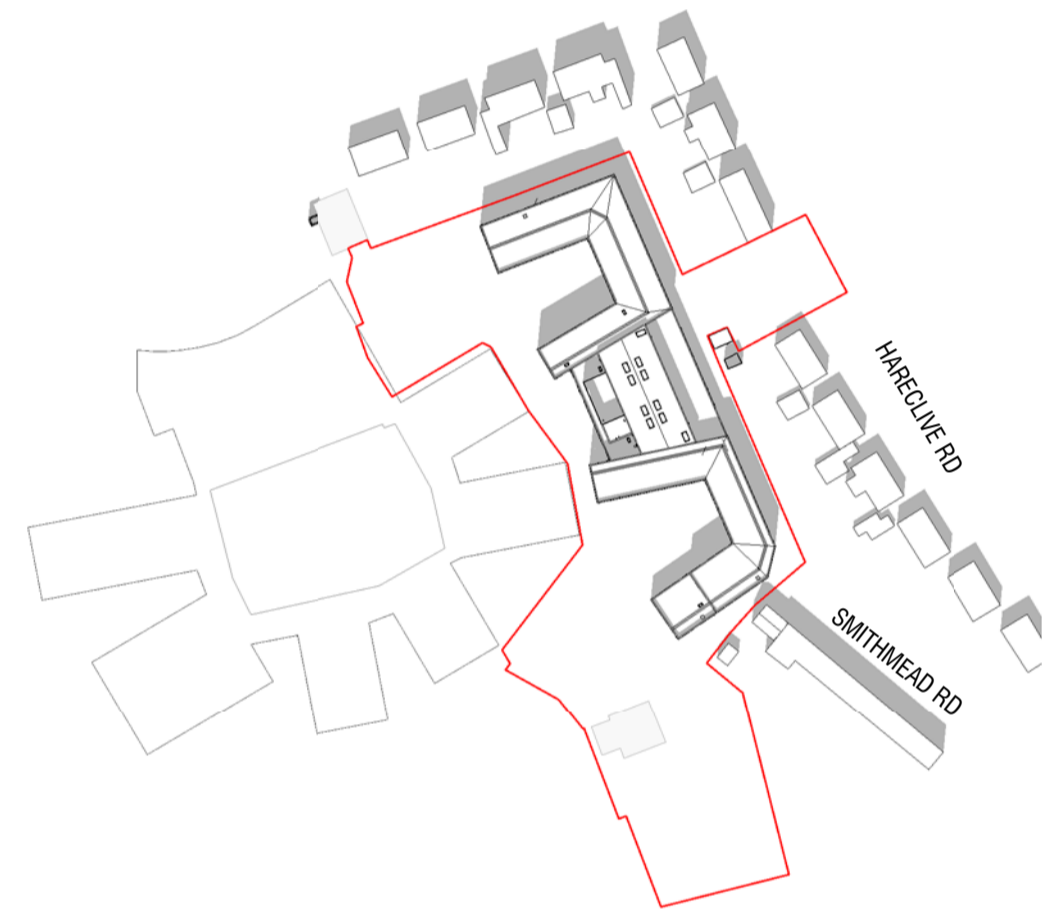
7 Dec 21st 10am
 TP(00)007 1 : 2000



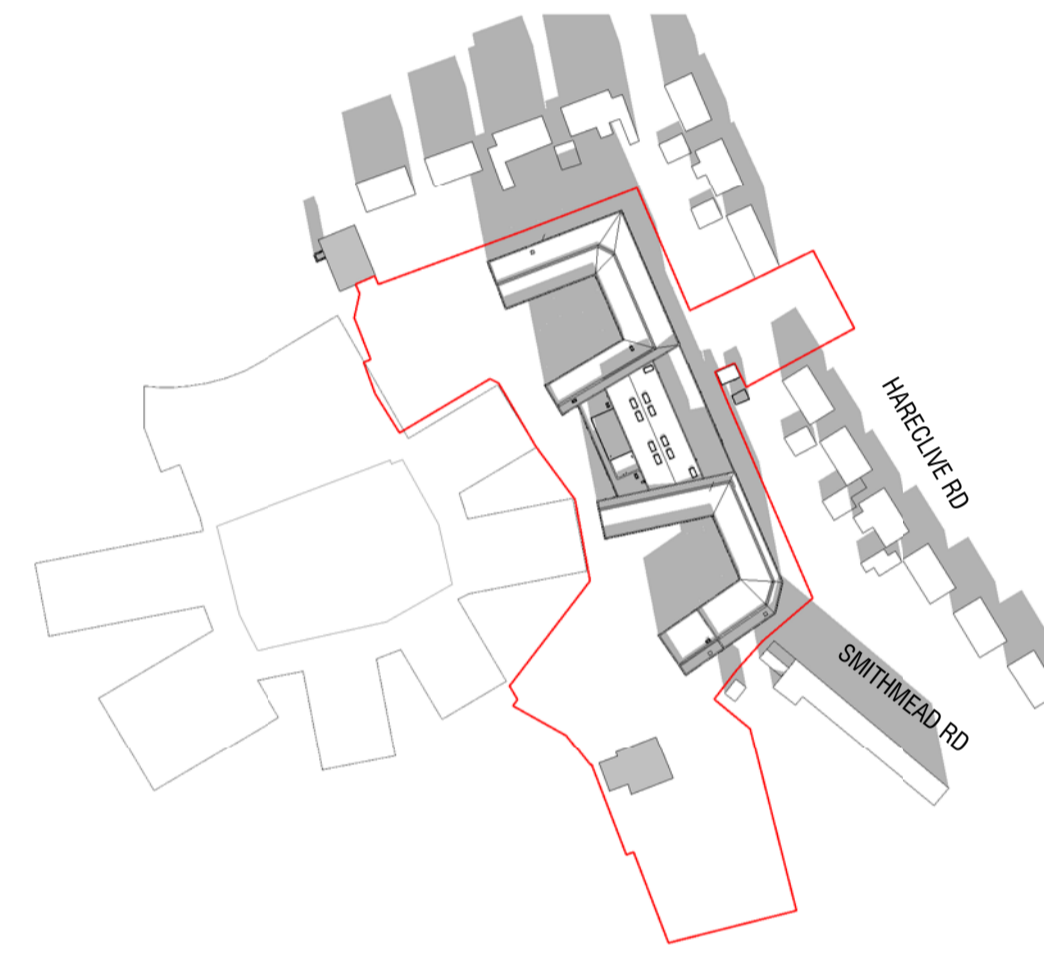
10 March 21st 9am
 TP(00)007 1 : 2000



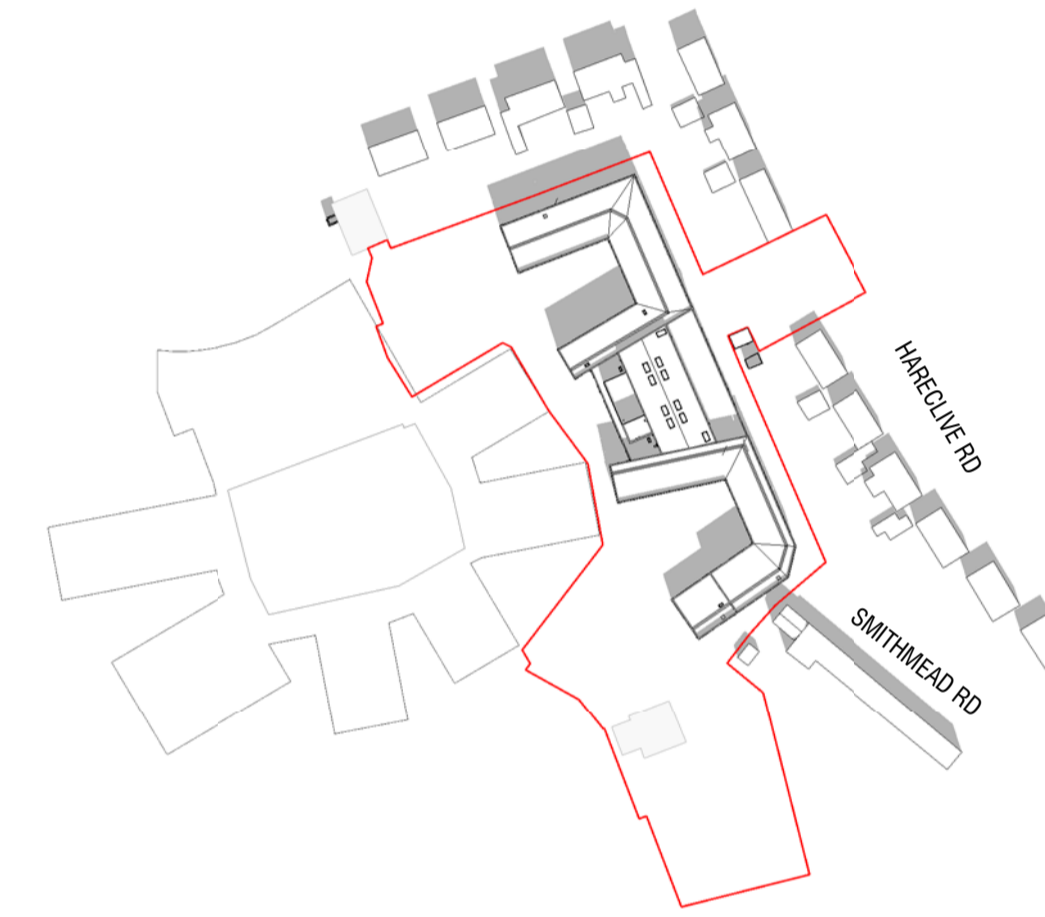
2 June 21st 1pm
 TP(00)007 1 : 2000



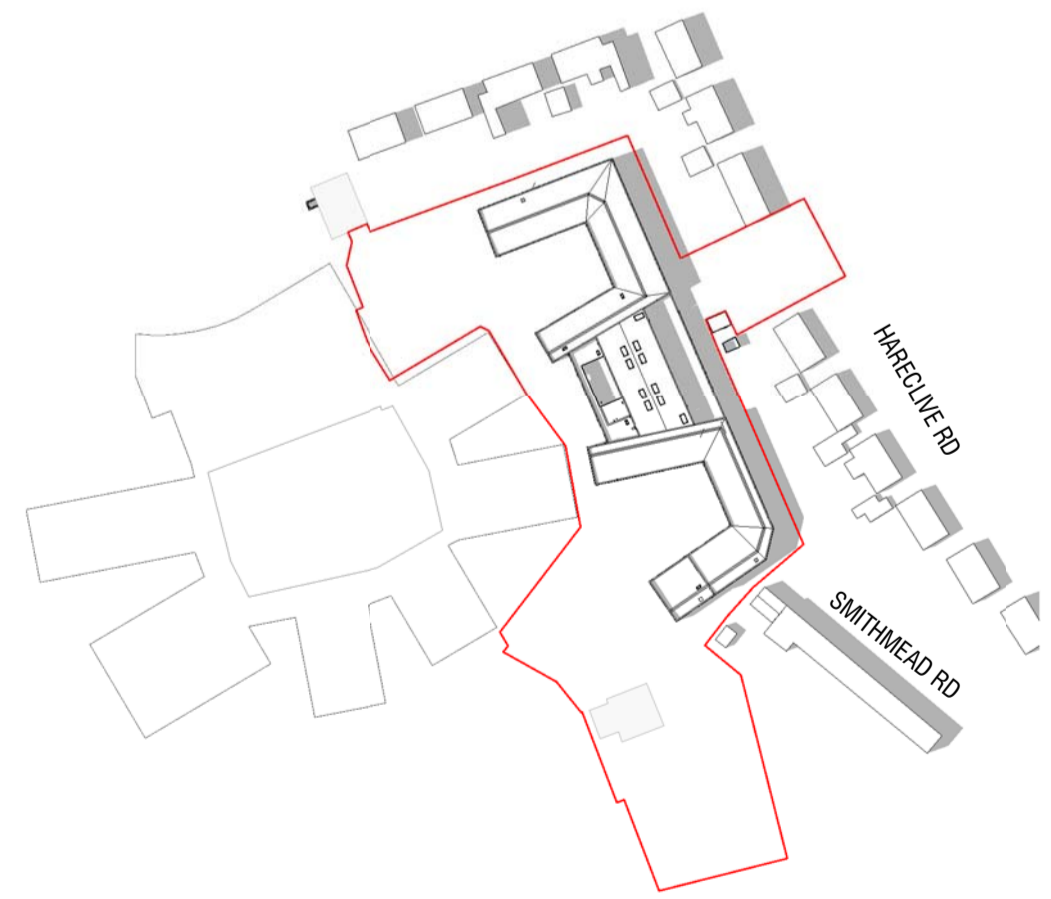
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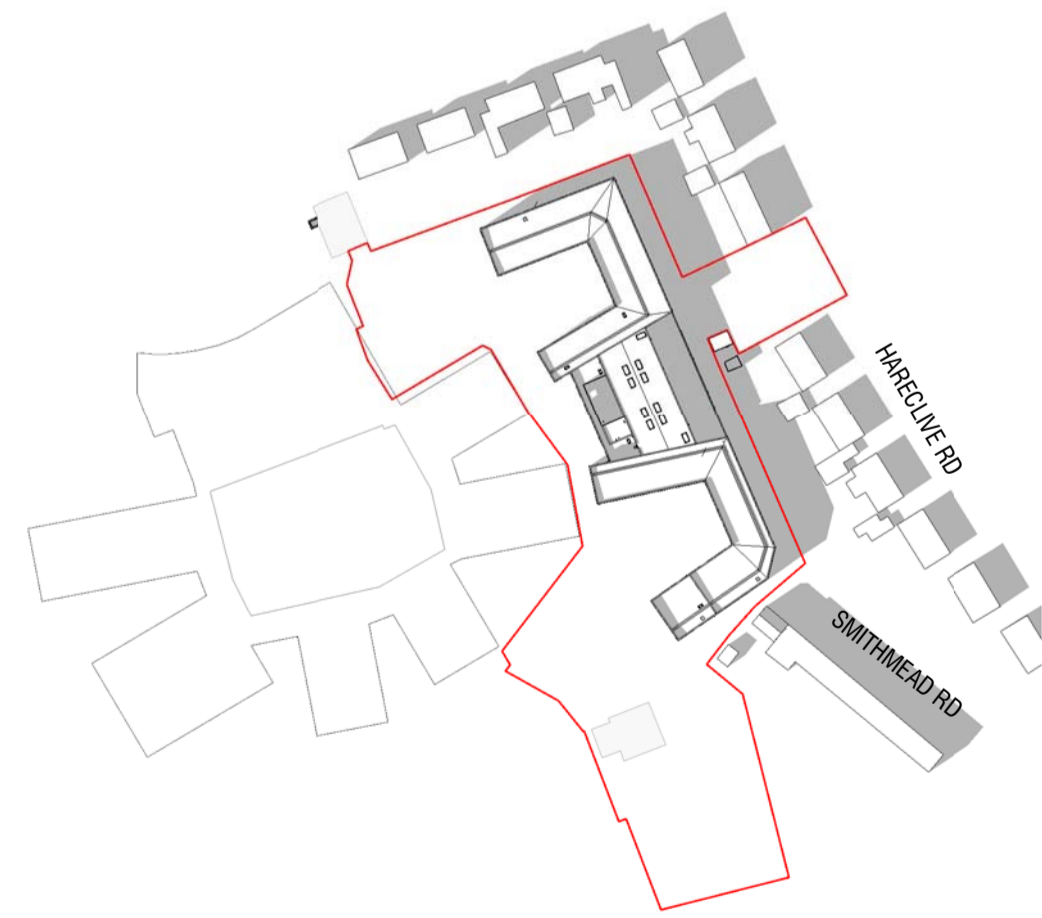
8 Dec 21st 12pm
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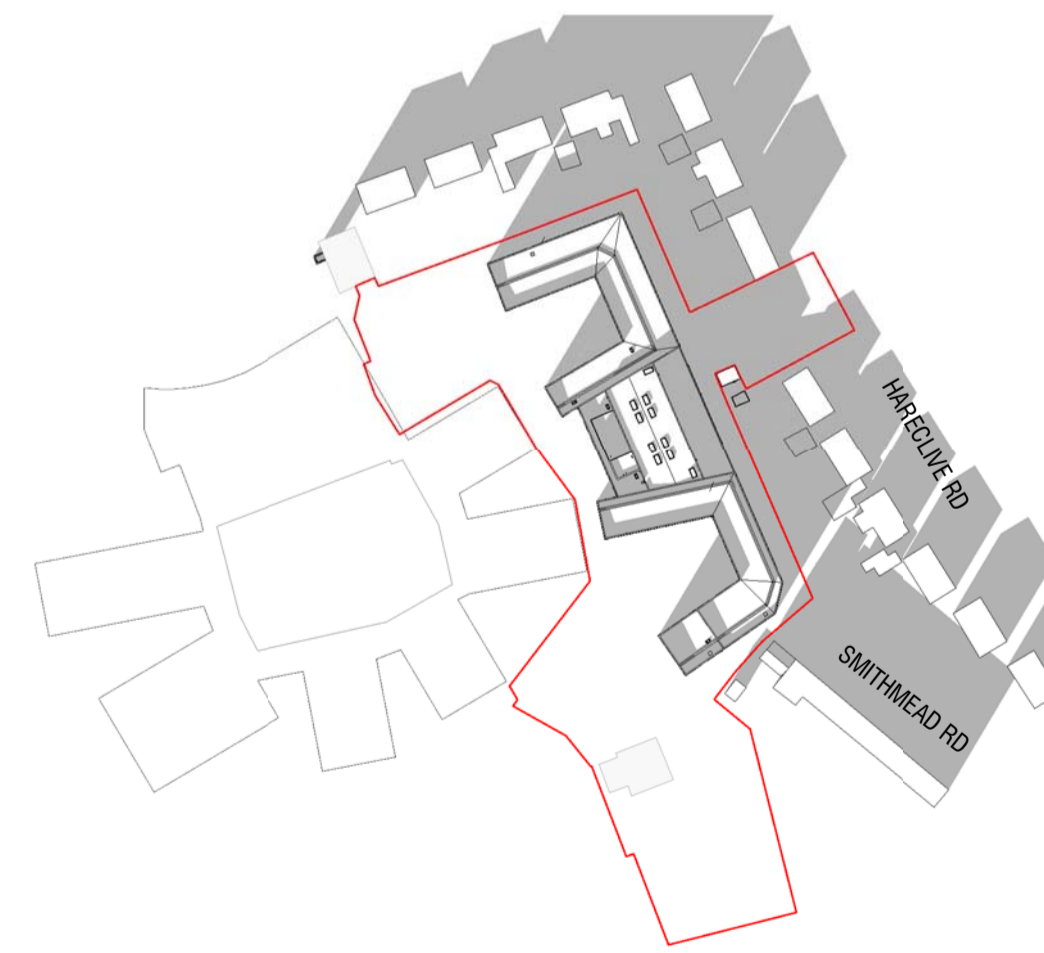
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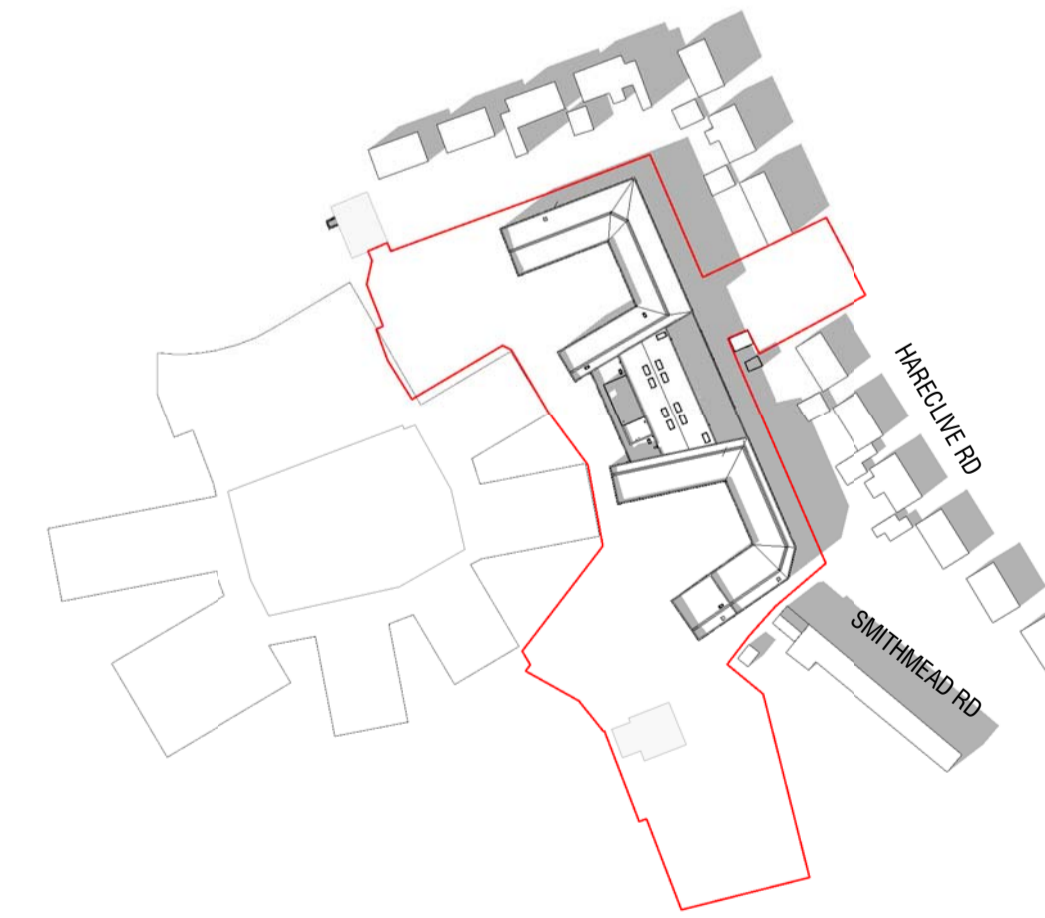
3 June 21st 5pm
 TP(00)007 1 : 2000



6 Sep 21st 4pm
 TP(00)007 1 : 2000



9 Dec 21st 3pm
 TP(00)007 1 : 2000



12 March 21st 4pm
 TP(00)007 1 : 2000

Revision	Description	Date	Drawn
SU 4	Planning Submission	25/05/18	ECS
SU 3	Draft Planning Submission	22/05/18	ECS
2	Revision for TP, Parapet height and window flashing	11/08/17	AK/ES

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 W scottbrownrigg.com

Client's Name
Galliford Try

Job Title
Merchants' Academy School and Venturers' Academy ASC School

Drawing Title
Annual Shadow plan

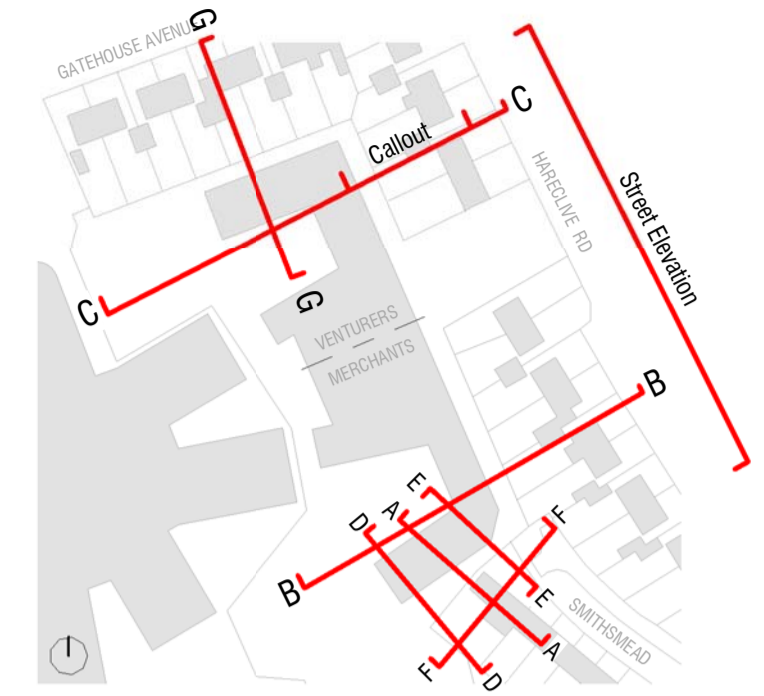
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16998 TP(00)007

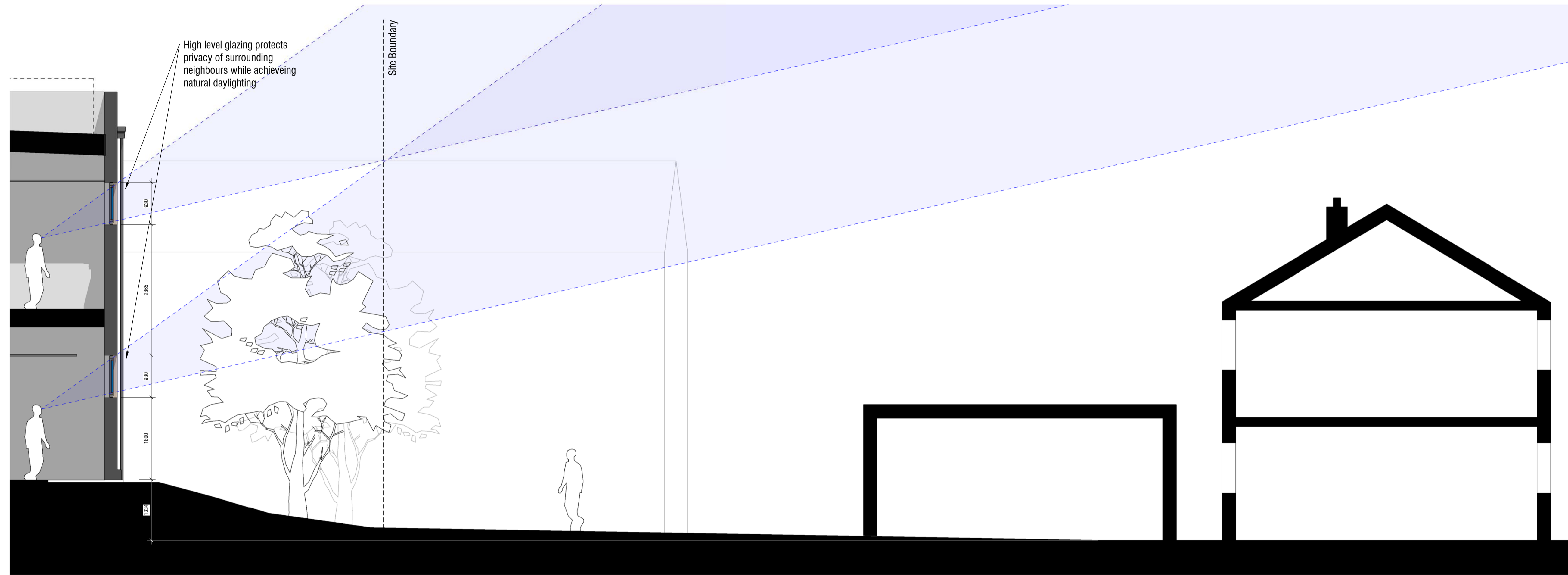
Status
PLANNING

Rev
4

Figured dimensions only are to be taken from this drawing. All dimensions are to be checked on site before any work is put in hand.



Callout from Section C-C, refer to TP(00)005 for full section



SU 3	Planning Submission	25/05/18	ECS
SU 2	Draft Planning Submission	22/05/18	ECS
SU 1	Eyesight Levels	03/05/18	ECS
Revision	Description	Date	Drawn



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Client's Name
Galliford Try

Job Title
Merchants' Academy School and Venturers' Academy ASC School

Drawing Title
Eyesight Levels

Scale
As indicated @ A1

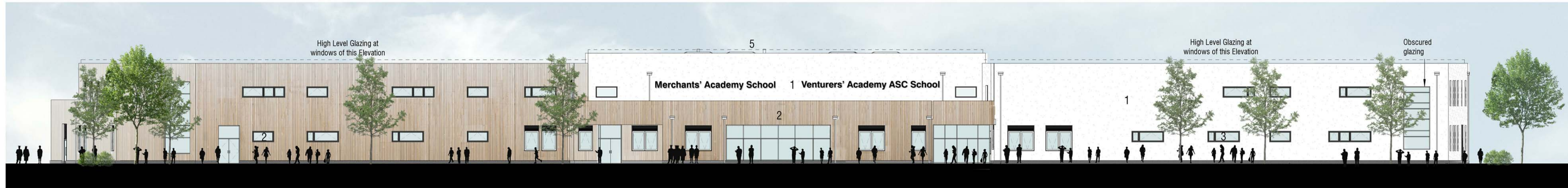
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Drawing No
TP(00)009

Status
PLANNING

Rev
3

1 Section C-C - Callout 1
TP(00)009 1 : 50



East Elevation - Front
1:200 @ A1



West Elevation - Rear
1:200 @ A1



North Elevation - Venturers' ASC Academy
1:200 @ A1



South Elevation - Merchants' Academy
1:200 @ A1



Materials Key

1. White Render with brick plinth
2. Timber Cladding with brick plinth
3. Aluminium windows (frame colour RAL 7024 TBC)
4. Full height aluminium windows with louvers (frame colour RAL 7024 TBC)
5. Railings, details subject to agreement with the LPA
6. Powder coated aluminium window reveals, cills and soffits (RAL 5012 TBC)
7. White Render window reveals
8. Timber Cladding window reveals

Revision	Description	Date	Drawn
SU 9	Planning Submission	25/05/18	ECS
SU 8	Draft Planning Submission	22/05/18	ECS
7	Railings Planning Update	12/10/17	AK
6	Updated Planning Elevations	21/09/17	ES



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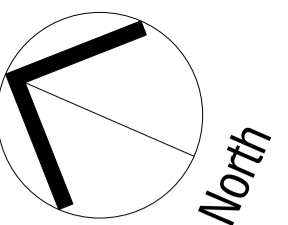
Drawing Title
Key Elevations

Scale
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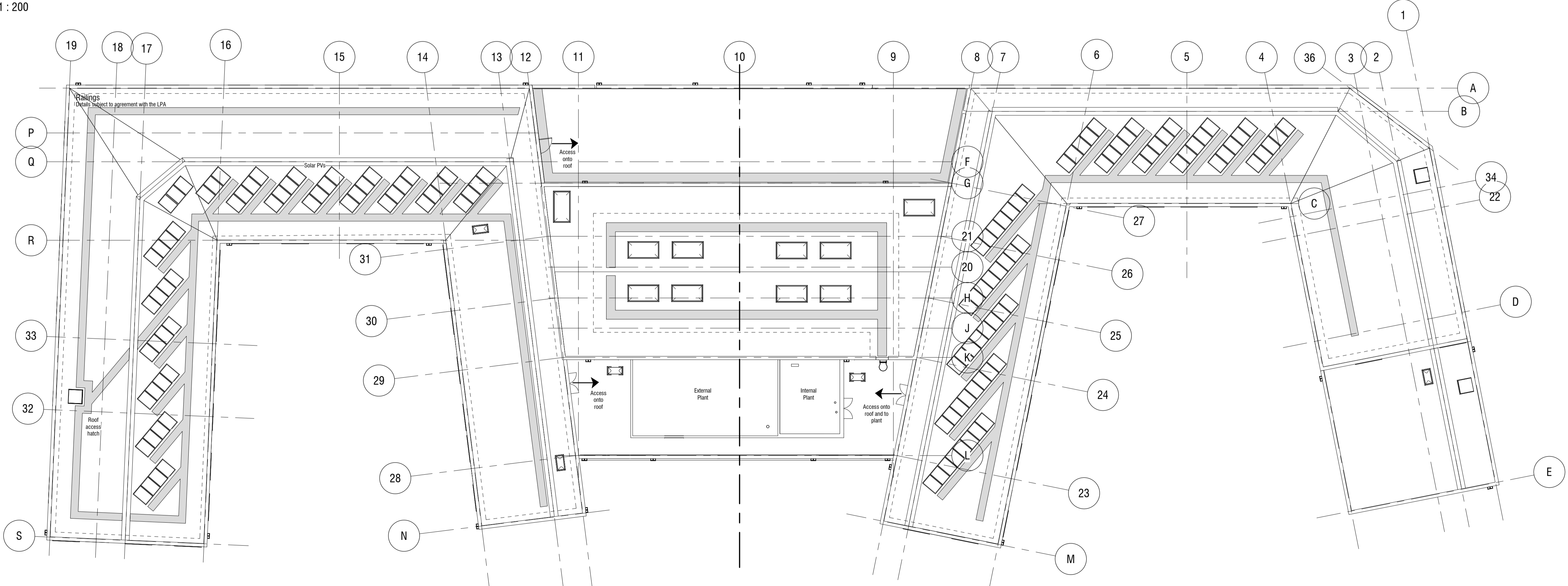
Drawing Number Drawing No
16998 TP(11)002

Status Rev
PLANNING 9

Figured dimensions only are to be taken from this drawing. All dimensions are to be checked on site before any work is put in hand.



1 First Floor Plan
TP(10)002 1 : 200



2 Roof Plan
TP(10)002 1 : 200

Revision	Description	Date	Drawn
SU 8	Planning Submission	25/05/18	ECS
SU 7	Draft Planning Submission	22/05/18	ECS
6	Railings Planning Update	12/10/17	AK
5	Updated Landscaping	21/09/17	ES



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Client's Name
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Job Title
Merchants' Academy School and Venturers' Academy ASC School

Drawing Title
First Floor & Roof Plan

Scale
1 : 200 @ A1

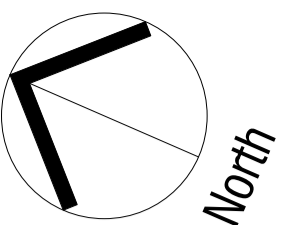
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Drawing No
TP(10)002

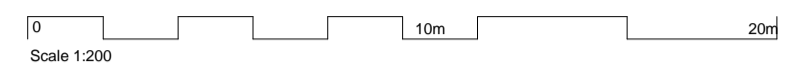
Status
PLANNING

Checked
SU

Rev
8



1 Ground Floor Plan
TP(10)001 1 : 200



Revision	Description	Date	Drawn
SU 6	Planning Submission	25/05/18	ECS
SU 5	Draft Planning Submission	22/05/18	ECS
4	Minor adjustments to Window Setting-out	26/07/17	SU
3	Planning Update	25/07/17	SU



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Galliford Try

Job Title
**Merchants' Academy School and
Venturers' Academy ASC School**

Drawing Title
Ground Floor Plan

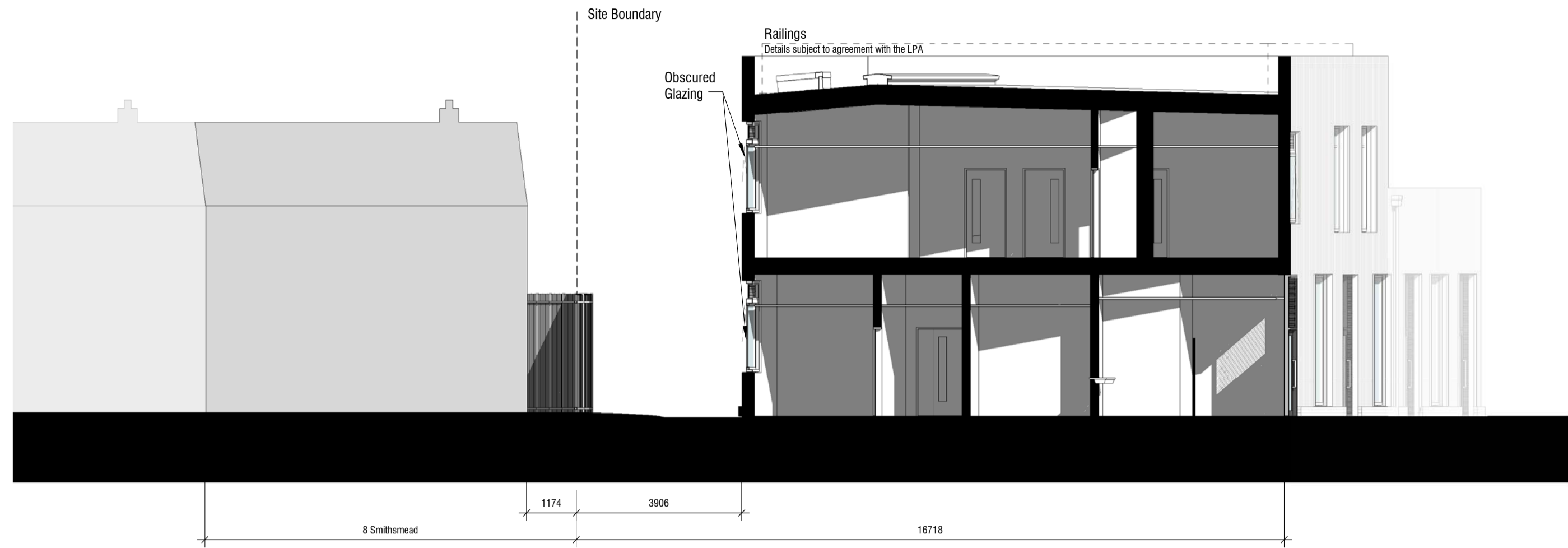
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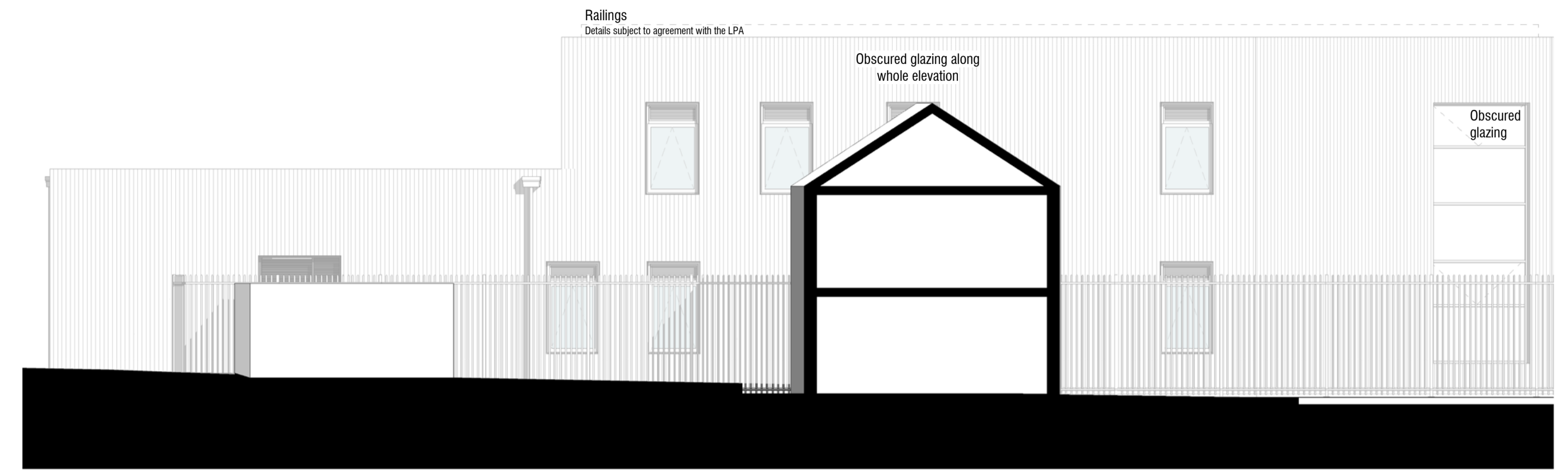
Drawing No
TP(10)001

Status
PLANNING

Rev
6



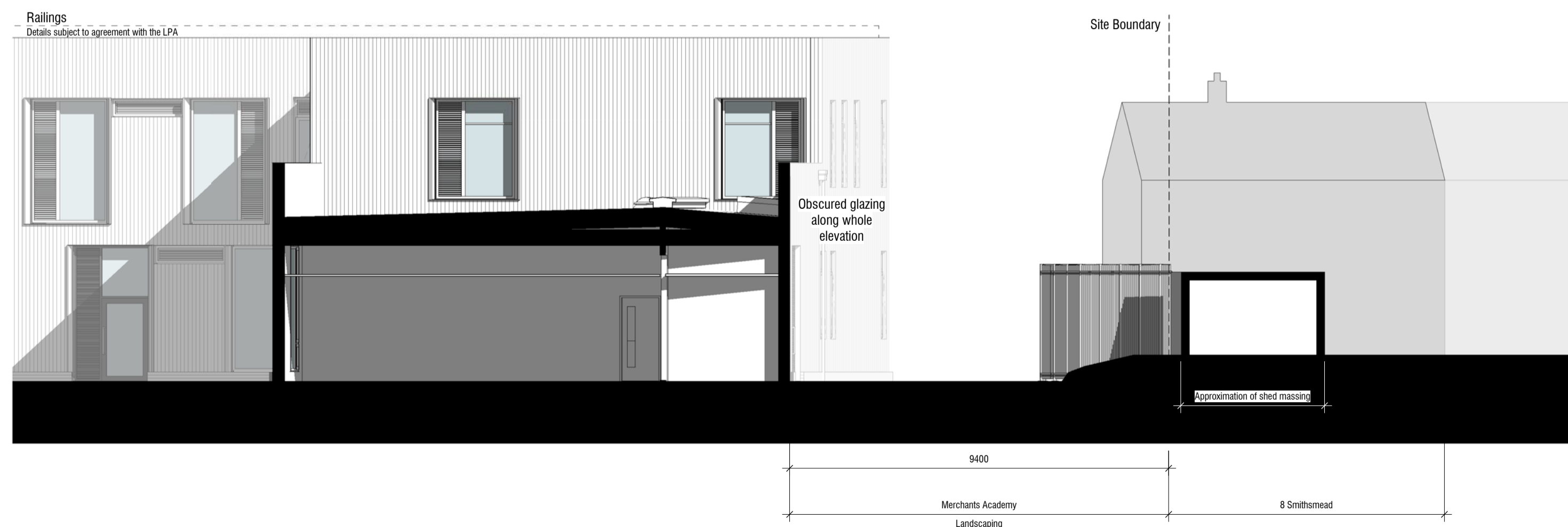
1 Section D-D Front Elevation of House
 TP(00)006 1 : 100



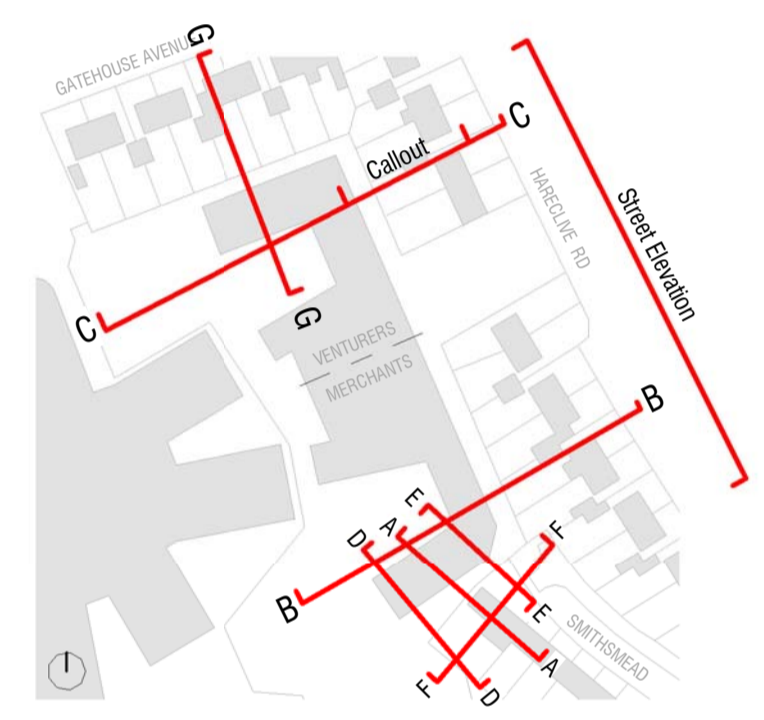
3 Section F-F Elevation towards school
 TP(00)006 1 : 100



4 Section A-A Against Gable End
 TP(00)006 1 : 100



2 Section E-E Through Garden, Shed and Teaching Block
 TP(00)006 1 : 100



Revision	Description	Date	Drawn
SU 5	Planning Submission	25/05/18	ECS
SU 4	Draft Planning Submission	22/05/18	ECS
3	Railings Planning Update	12/10/17	AK
2	Updated Planning Elevations	21/09/17	ES

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Client's Name
Galliford Try

Job Title
Merchants' Academy School and Venturers' Academy ASC School

Drawing Title
Site Cross Sections 2

Scale
As indicated @ A1

Drawing Number
16998

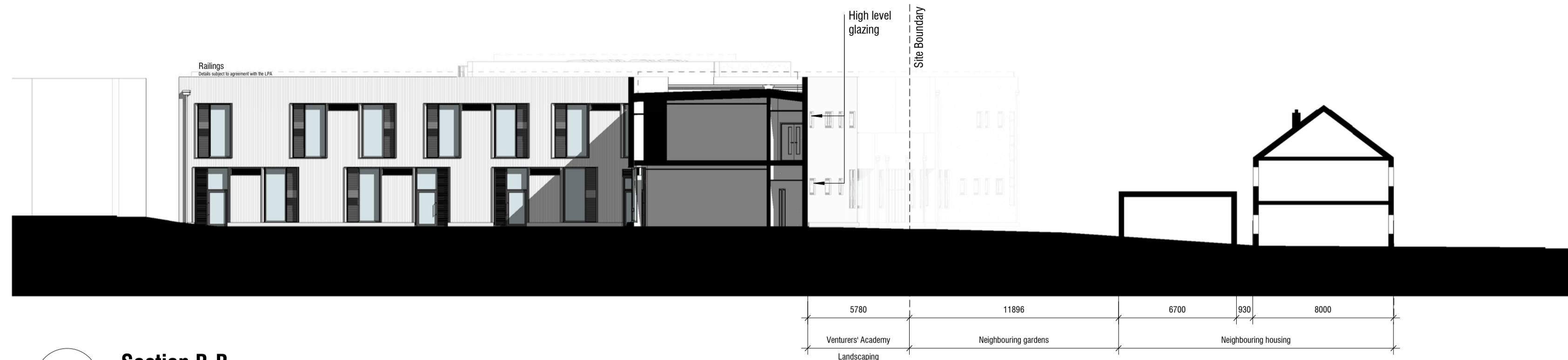
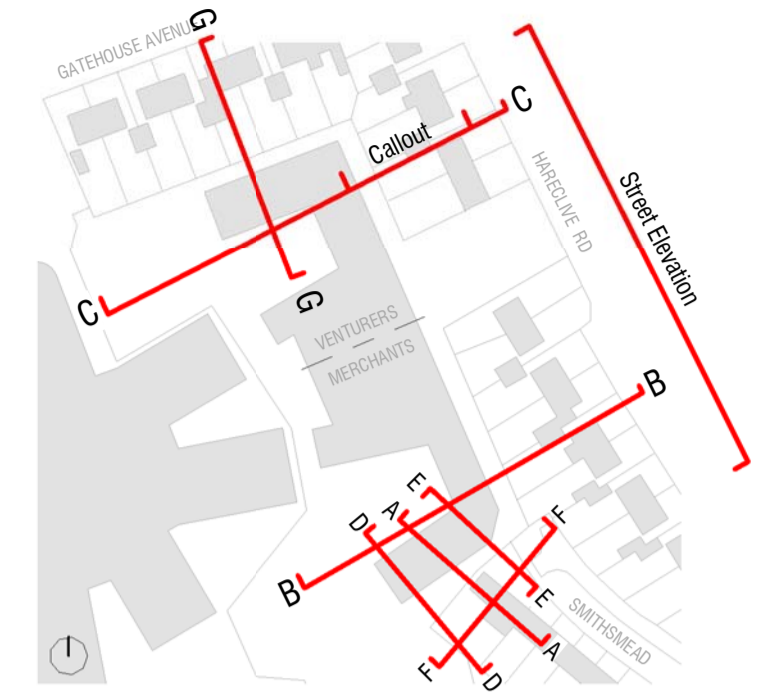
Drawing No
TP(00)006

Status
PLANNING

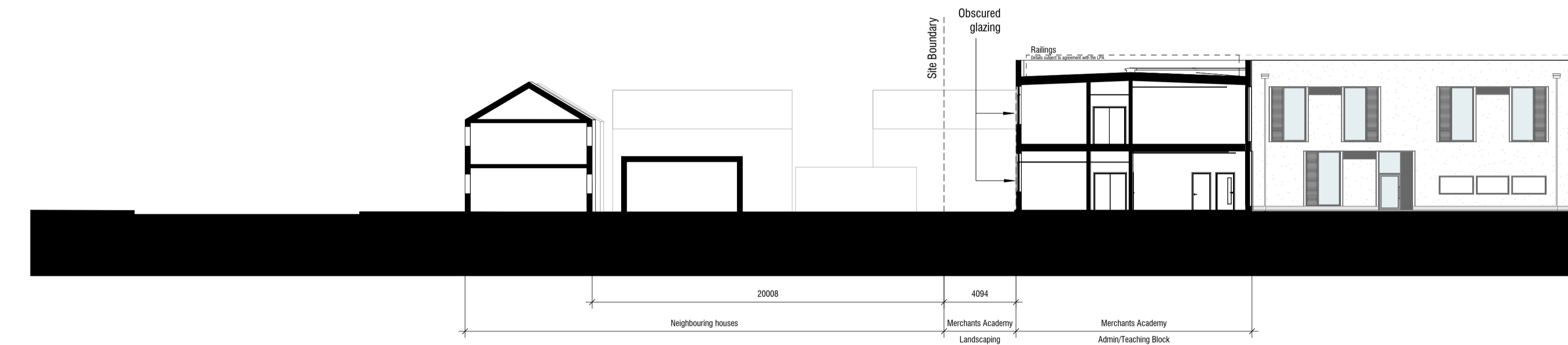
Checked
 SU

Rev
5

Figured dimensions only are to be taken from this drawing. All dimensions are to be checked on site before any work is put in hand.



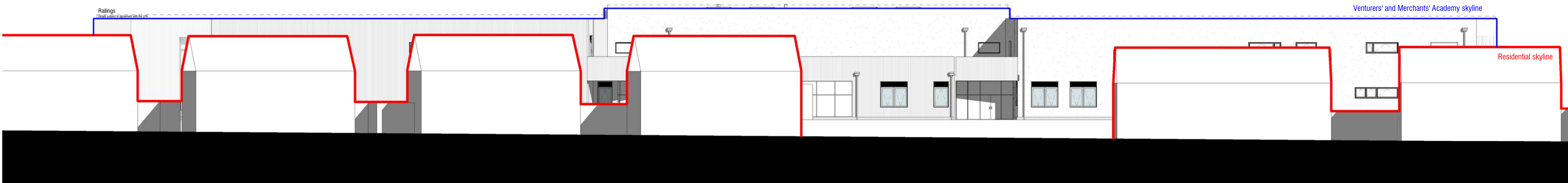
2 Section B-B
TP(00)005 1 : 200



4 Section G-G
TP(00)005 1 : 200



3 Section C-C
TP(00)005 1 : 200



1 Street Elevation
TP(00)005 1 : 200

Revision	Description	Date	Drawn
SU 6	Planning Submission	25/05/18	ECS
SU 5	Draft Planning Submission	22/05/18	ECS
4	Rallings Planning Update	12/10/17	AK
3	Planning Chamber Option	18/09/17	ES



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Client's Name
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Job Title
**Merchants' Academy School and
Venturers' Academy ASC School**

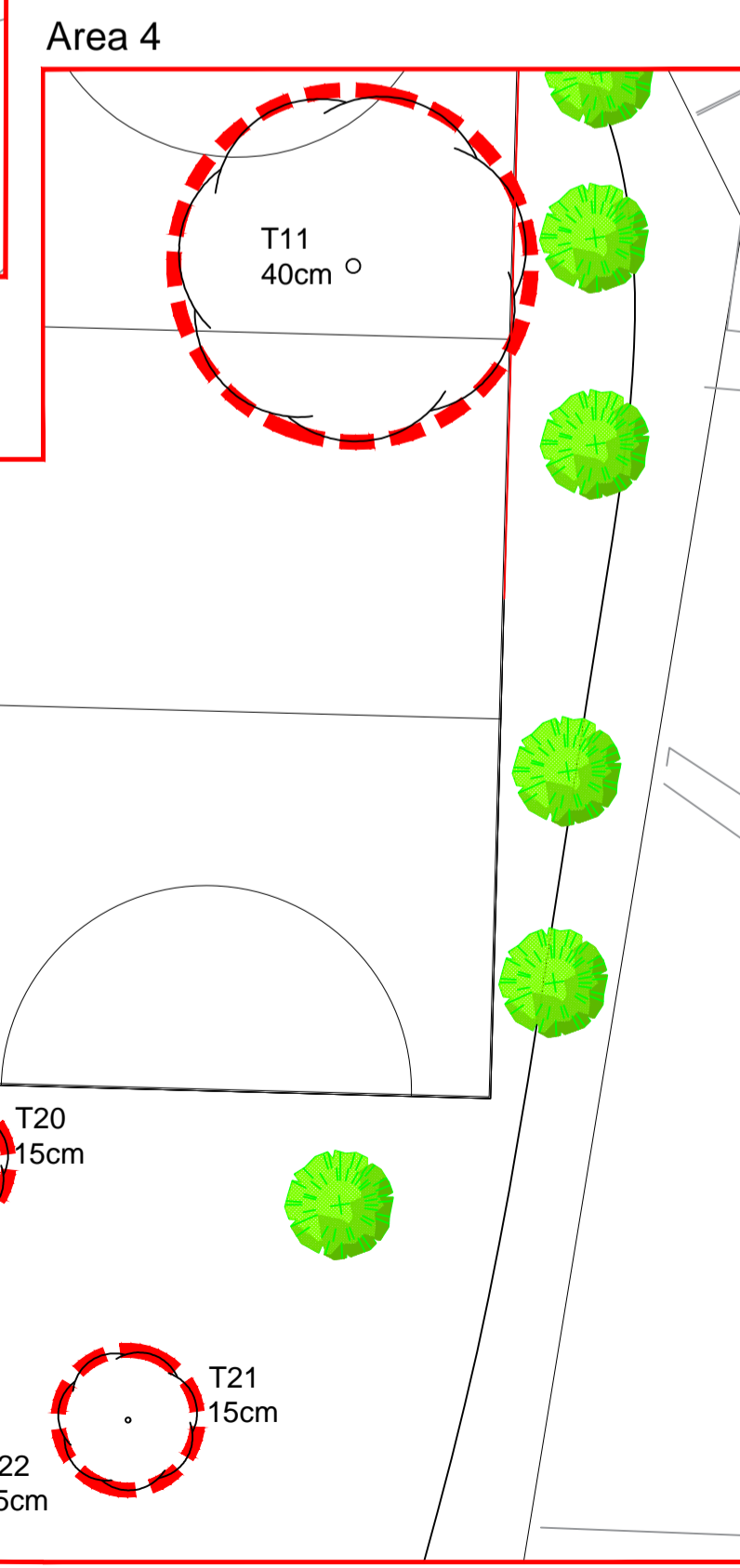
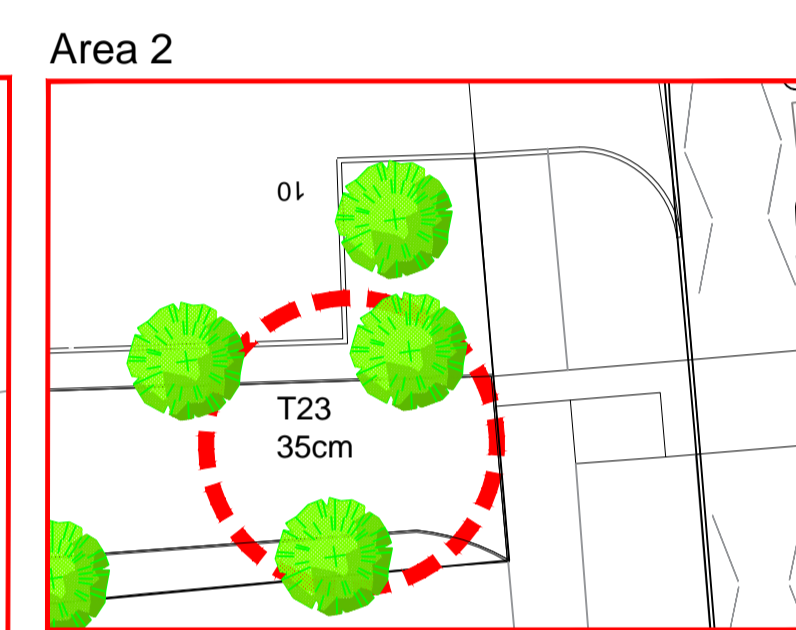
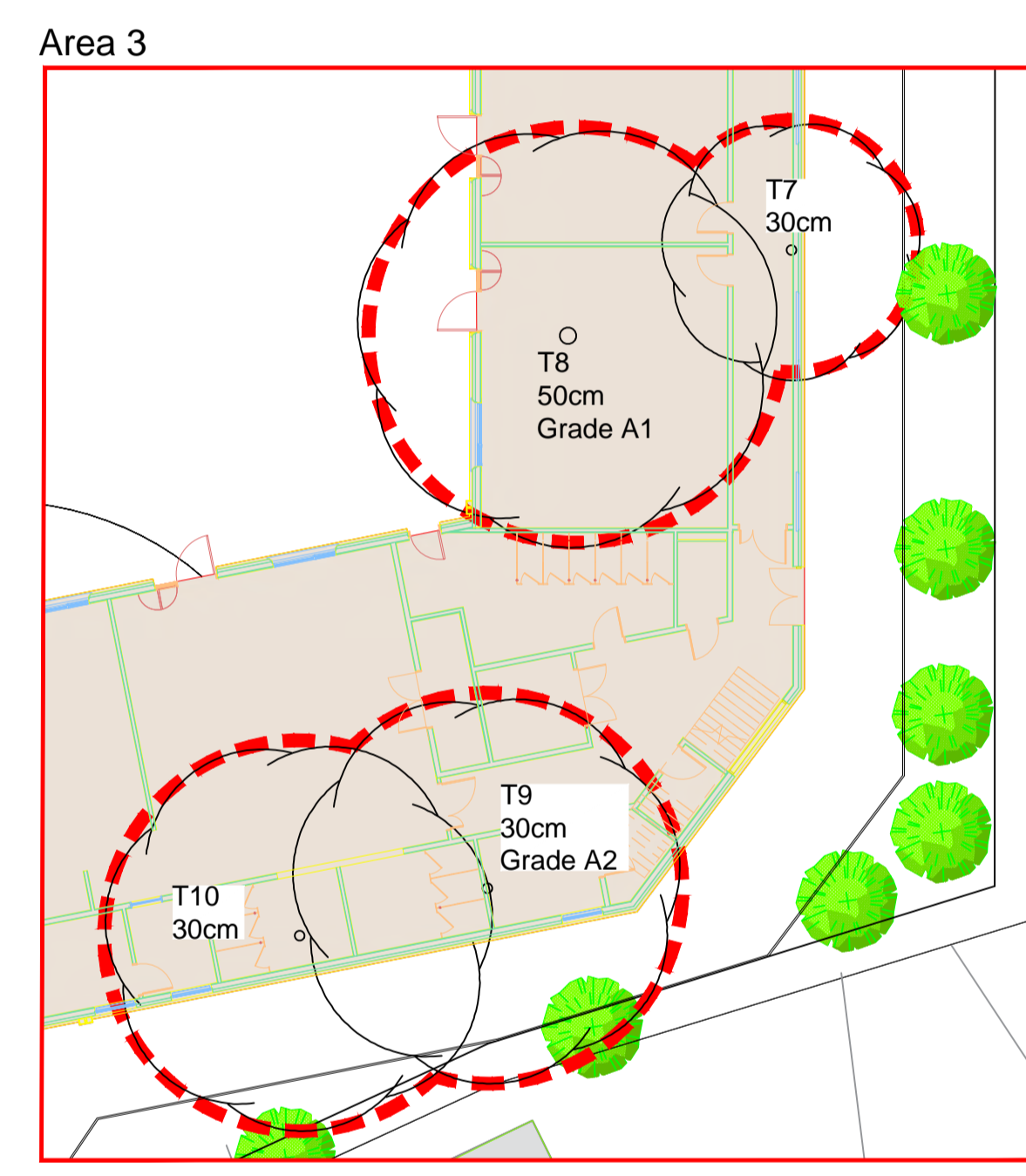
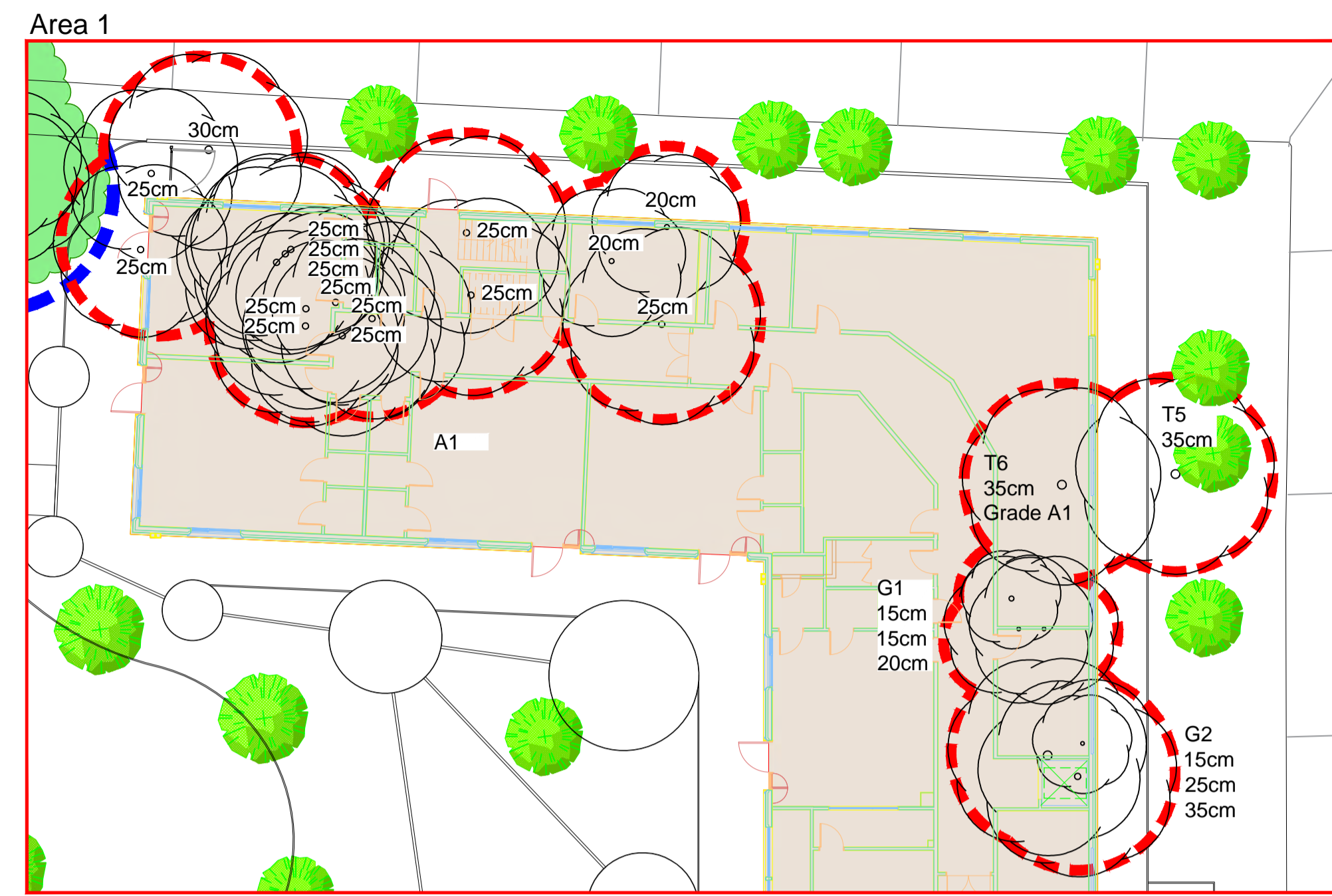
Drawing Title
**Site Cross Sections and Street
Elevation**

Scale
As indicated @ A1

Drawing Number Drawing No
16998 TP(00)005

Status Rev
PLANNING 6

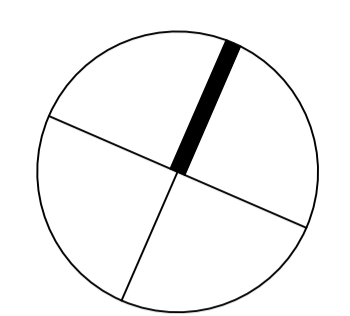
Overall layout plan



Arboricultural Survey Reference	Existing Tree Size - Diameter (cm)	No. removed	Replacement factor	Number of Replacements
G1,G1,G2,T20,T21,T22	15	6	1	6
A1,A1,G1	20	3	2	6
A1x13,G1	25	14	2	28
A1,T7,T9,T10	30	4	3	12
T5,T6,G2,T23	35	4	3	12
T11	40	1	4	4
T8	45	0	4	0
T8	50	1	5	5
Total		33		73

Tree No.	Species	Tree Height (m)	Age Class	Condition Physiological/Structural	Safe Useful Life Expendancy (years)	B5 Category	Observations
T5	Field Maple	8	Middle Age	Good/Fair	20-40	B3	Triple-stemmed specimen, Two stems naturally graft at ~1.3m so main stem diameter measured at ~1.0m
T6	Ash	12	Middle Age	Good/Good	>40	A1	Well balanced specimen with good future potential.
T7	Sycamore	10	Young	Good/Good	20-40	B1	Tree becoming dominated by Ash T8.
T8	Ash	12	Middle Age	Good/Good	>40	A1	Well balanced specimen with good future potential. Crown coalescing with Norway Maple T10 adjacent.
T9	Ash	13	Middle Age	Good/Good	>40	A2	Well balanced specimen with good future potential. Crown coalescing with Norway Maple T10 adjacent.
T10	Norway Maple	11	Middle Age	Good/Fair	>40	B2	Tree features congested point of main crown break, crown coalescing with Ash T9 adjacent.
T11	Ash	10	Middle Age	Poor/Poor	10-20	C1	Tree features extensive dieback on eastern side of crown, tree has limited future potential.
T20	Ash	6	Young	Good/Good	>40	C1	Young specimen.
T21	Ash	6	Young	Good/Good	>40	C1	Young specimen, scuff on southern side of main stem at ~0.7m.
T22	Ash	7	Young	Good/Good	>40	C1	Young specimen.
T23	Cherry	7.5	Middle Age	Good/Fair	20-40	C1	Ornamental specimen, full inspection of tree not possible. Boundary copse which has been crown lifted. Trees form effective screen from properties to the north and area has good potential as Forest School teaching space. Some Ivy present.
A1	Cherry, Field Maple, Willow, Oak	<13	Middle Age	Fair to Good/Fair to Good	>40	B2	Group comprising one Cherry and two Blackthorn stems, Blackthorn is considered inappropriate for education environment. Cherry is twin-stemmed.
G1	Cherry, Blackthorn	<7	Young	Fair/Fair	20-40	C1	Three trees in a triangular arrangement. One tree is single stemmed, one is twin-stemmed and one is tripesstemmed. No individually outstanding stems within the group, but collectively they form a reasonable feature.
G2	Cherry	<9	Young	Good/Fair to Good	>40	B2	

NOTES:
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- Legend-**
- Proposed trees
 - Trees to be retained
 - Root Protection Zone
 - Trees to be removed
 - Tree diameter taken from topographical survey

Note -
Refer to Arboricultural Tree Constraints Plan TH/X1455/1116

Planning



P5. Layout updated	25.05.18 MA
P4. Table added to with survey information	15.02.18 ZC
P3. Updated layout to include art in the landscape	22.08.17 ZC
P2. Updated landscape	08.08.17 ZC
P1. Updated layout for CP	01.06.17 RK

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PROJECT	Merchants' and Venturers' School		
SUBJECT	Trees Removed, Retained, Proposed		
DATE	23.06.17	DRAWN BY	RK
SCALE	1:500 @ A1	CHECKED BY	IN
DRAWING NUMBER	MAB-HED-L-XX-DR-2008	REVISION	P5
DRAWING STATUS	A - PRELIMINARY B - ISSUED FOR PLANNING C - ISSUED FOR DESIGN INFORMATION D - ISSUED FOR TENDER		
E - ISSUED FOR APPROVAL	F - ISSUED FOR CONSTRUCTION	G - AS BUILT	